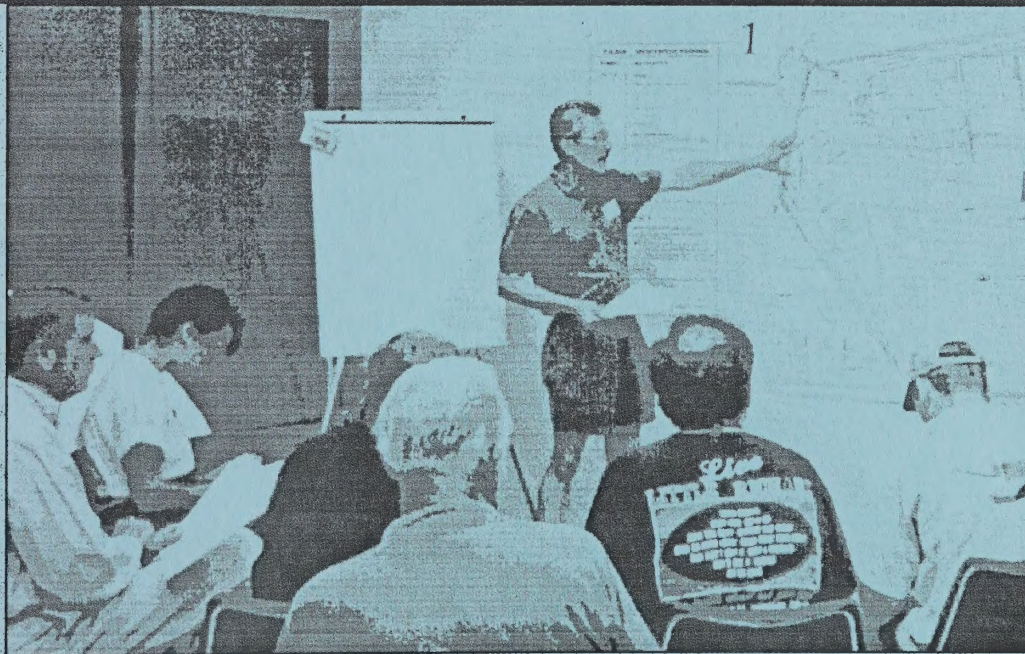


Whisman Area Plan Looking Ahead



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A RESOLUTION CONCERNING THE WHISMAN AREA PLAN

WHEREAS, on July 2 and 13, 1997, the Environmental Planning Commission and staff held a series of public meetings and hearings to discuss and develop the Whisman Area Plan; and

WHEREAS, on October 20, 1997, the Commission adopted the Whisman Area Plan with certain amendments;

NOW, THEREFORE, BE IT

Final

RESOLVED, That the City Council hereby approves and adopts the Final Whisman Area Plan as a resolution of the City of Menlo Park, California, and that the City Council hereby certifies that the Whisman Area Plan is in compliance with the requirements of the California Environmental Quality Act, as amended, and the California Public Resources Code, as amended.

Whisman Area Plan

Approved by the City Council on December 1, 1997, and by the City Council on December 1, 1997.

Attest: The Mayor, City of Menlo Park, California, do hereby certify that the Whisman Area Plan is in compliance with the requirements of the California Environmental Quality Act, as amended, and the California Public Resources Code, as amended.

The foregoing Resolution was read and approved by the City Council on December 1, 1997, and the City Council hereby certifies that the Whisman Area Plan is in compliance with the requirements of the California Environmental Quality Act, as amended, and the California Public Resources Code, as amended.

ATTEST: City Clerk, City of Menlo Park, California, do hereby certify that the Whisman Area Plan is in compliance with the requirements of the California Environmental Quality Act, as amended, and the California Public Resources Code, as amended.

ATTEST: City Clerk

ATTEST: City Clerk

ATTEST: City Clerk

ATTEST:

ATTEST: City Clerk

ATTEST: City Clerk

ATTEST: City Clerk

December 1997

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 16189
SERIES 1997

A RESOLUTION ADOPTING THE WHISMAN AREA PLAN

WHEREAS, on July 2 and 16, 1997, the Environmental Planning Commission held a duly noticed public hearing and thereafter forwarded its recommendation to the City Council that the Whisman Area Plan be adopted; and

WHEREAS, on October 15, 1997, having given notice, the City Council held a public hearing to consider adopting said Area Plan.

NOW, THEREFORE, BE IT RESOLVED AS FOLLOWS:

1. FINDINGS: The City Council hereby finds and determines that this Area Plan will not have a significant effect on the environment, as is more particularly described in the Negative Declaration attached hereto as Exhibit "A," which is incorporated by reference herein.

2. DETERMINATIONS:

a. Negative Declaration. The City Council hereby approves the Negative Declaration attached hereto as Exhibit "A," which is incorporated by reference herein.

b. The Whisman Area Plan, as is more particularly described in Exhibit "B," attached hereto and incorporated by reference herein, is hereby adopted.

The foregoing Resolution was regularly introduced and adopted at a Regular Meeting of the City Council of the City of Mountain View, duly held on the 9th day of December, 1997, by the following vote:

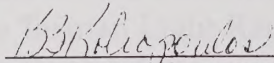
AYES: Councilmembers Ambra, Faravelli, Figueroa, Noe, Stasek,
Zoglin and Mayor Kleitman

NOES: None

ABSENT: None

NOT VOTING: None

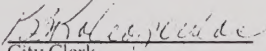
ATTEST:

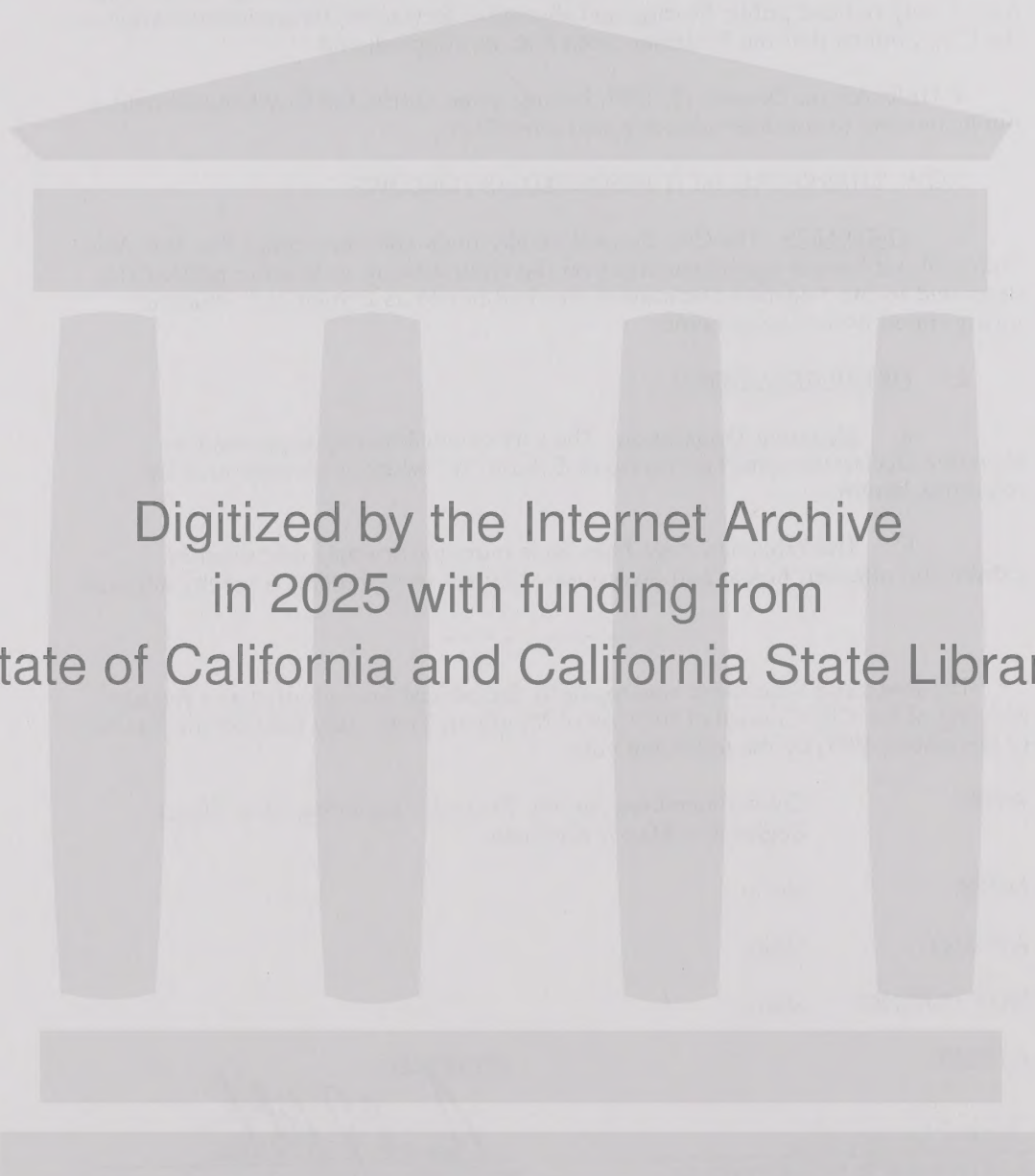

KATHERINE B. KOLIOPOULOS
CITY CLERK

APPROVED:


JOSEPH S. KLEITMAN
MAYOR

I do hereby certify that the foregoing resolution was passed and adopted by the City Council of the City of Mountain View at a Regular meeting held on the 9th day of December 1997 by the foregoing vote.


City Clerk
City of Mountain View



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Whisman Area Plan

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ATTACHMENTS AND EXHIBITS

Attachment A	Evandale Precise Plan
Exhibit A1	Sign Program for Area A
Exhibit A2	Excerpt—Zoning Ordinance—R3* District Regulations
Attachment B	Parks and Open Space Element
Attachment C	Community Appearance
Attachment D	Amendments to ML Zone
Attachment E	Ellis-Middlefield Streetscape Guidelines

WHISMAN AREA PLAN

In the mid 1990s, the Whisman area of Mountain View, which is bounded by U.S. 101, State Route 237, Central Expressway and State Route 85, suddenly began undergoing major changes. The industrial area began evolving into a modern office and research park. The residential neighborhood was maturing and becoming more concerned about growth. And the arrival of the Tasman Light Rail line started to stimulate new interest and activity. Concerned about how to respond to these changes and how to blend the old with the new, the City Council directed the Environmental Planning Commission to prepare a plan for the Whisman area. Over a period of a year in 1996-97, the Commission held numerous meetings involving many residents and property owners to discuss and develop responses to these issues. The result of their effort is the Whisman Area Plan. The Draft Plan was reviewed and approved, with some revisions, by the City Council in December 1997.

As the Commission hammered out its recommendations on how to prepare for change, a vision of an ideal future for the Whisman Area began to emerge. This vision is presented below. The policies, projects and proposals in the Whisman Area Plan are designed to help achieve this future.

A Vision for the Whisman Area

In the future....

- ◆ *The Whisman Area will continue to have its own unique identity. It will be a crossroads between an established, but still evolving, residential community, and one of Mountain View's two high-technology job centers.*
- ◆ *The Whisman Area's past, as a birthplace of the semiconductor industry, will be remembered with an historical display, exhibit or other landmark feature in a highly visible, public location such as a Light Rail station.*
- ◆ *The geographical center of the community where Whisman and Middlefield Roads intersect will be an attractive, interesting and commercially vibrant place for residents, workers and shoppers to meet and interact.*
- ◆ *Whisman Road will provide a graceful transition, through landscaping and controlled traffic, from the places where people live to the places where people work.*
- ◆ *It will be a common sight in this area to see people walking—from the light rail stations to their jobs, from their jobs to restaurants and from their homes to nearby parks and stores.*
- ◆ *Parks and open spaces will be dispersed through the neighborhood. Trails will connect the residential and industrial areas with each other and with the other parts of the City along the Stevens Creek Trail.*
- ◆ *The two Light Rail stations will be active, lively and pleasant places to spend time.*



BACKGROUND

What Is an Area Plan?

An Area Plan is a set of objectives or policies for an area and the actions and programs needed to implement them. It is not a legal document like a General Plan, Precise Plan or zoning ordinance. It is a layer between the General Plan and the action tools (zoning, capital improvements, guidelines) used to implement City policy. It is aimed at the intermediate time frame: what can be accomplished in 5 to 10 years. It compiles in one place all of the desired changes for the area, whether they will be carried out by public agencies or private companies and individuals. An Area Plan permits a comprehensive view of a large area of the City.

In the process of developing this Area Plan, several of the implementing tools were also developed. These are the zone changes (the Evandale Precise Plan and the changes to the ML zone) and the streetscape guidelines. The City Council adopted the zone changes and the streetscape guidelines at the same time that it adopted the Whisman Area Plan. The Council also approved an amendment to the General Plan land use map in the Evandale Precise Plan area to identify mini park locations.

Why Do an Area Plan?

Development of both the residential and industrial parts of the Whisman Area began in the 1950s and 1960s. The 1960s was a period of strong growth and productivity as Fairchild Semiconductor, GTE, Raytheon and other pioneering electronic companies set up their manufacturing operations. Many new and simply designed industrial buildings were built. By the early 1980s, widespread soil and groundwater contamination, caused by chemicals used in the manufacture of silicon computer wafers, had been discovered in portions of the industrial area.¹ This led to the designation of a large part of the industrial area as a Superfund site, and cleanup activities began. Over the next 10 years, Fairchild moved out of Mountain View, leaving behind more than 40 acres of land, much of it vacant.

¹Background Report, "The Middlefield-Ellis-Whisman (MEW) Superfund Study Area," May 1, 1996.

The 1992 General Plan identified this area as one of the seven “areas of opportunity,” which were expected to undergo significant change and development in the next 10 to 15 years. Change was anticipated because of the large amounts of vacant land, the age of much of the existing industrial development, and the County’s decision to extend a Light Rail line into Mountain View using an existing railroad spur through the industrial area. The City Council allocated funds in Fiscal Year 1994-95 for a planning study of the industrial area that would consider housing, mixed-use and industrial redevelopment. (Other major projects delayed the start of the study to 1996.)

In 1995, 40 acres of GTE land, on the southern side of the industrial area, were rezoned for residential redevelopment. The 1992 General Plan housing forecasts had not anticipated housing on this property, and rezoning generated strong neighborhood interest. By 1995, surface cleanup of the Fairchild property was completed and the site was also ready for reuse. The economy was improving and developers began approaching the City about new office and research and development (R&D) development on the Fairchild site and on other prime properties. These proposals removed key tracts of land from consideration for possible rezoning to other uses. Concurrently, neighborhood residents were urging greater attention to their needs for parks and open space, traffic controls and code enforcement.

The decision was made in May 1996 to refocus the planning study from the Ellis-Middlefield industrial area to the larger Whisman Area. It was decided that the basic land use in the industrial area would not be changed. The extensive use of hazardous materials by businesses in the industrial area and the ongoing cleanup activities (including aboveground air stripping towers) raised questions about the compatibility of residential and mixed-use development. In addition, the strong demand for office and R&D space had the potential to make this area a “Class A” industrial park equal to the North Bayshore Area.

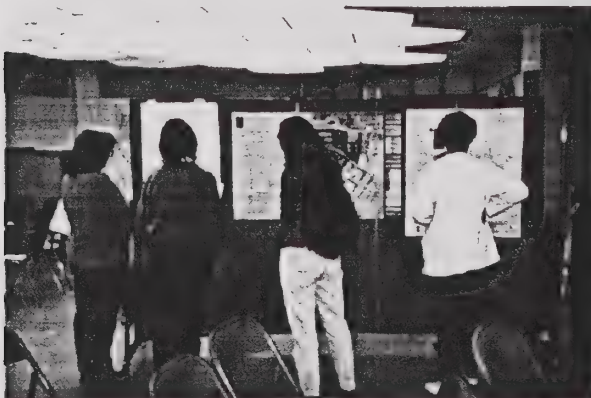
Since the planning issues are diverse, but do share a common geographical area, it was decided that an Area Plan, that could consider all of these issues at one time, should be done. Unlike a traditional General Plan study, that comprehensively addresses a full spectrum of community development issues, this plan retains most of the existing land use pattern and focuses on the locations and issues where change is occurring or where change is needed.

The Neighborhood and the Planning Process

The public planning process began in June 1996 when planning kits were hand-delivered to every house, apartment and condominium in the Middlefield-Whisman residential area. The planning kits were maps of the area with labels that respondents could glue onto the map. The labels said "open space," "restaurant," "clean up," "remove" and many other descriptive words that the resident/respondent could use to state his/her desires for the neighborhood. Sixty-five completed planning kits were returned to the City.

On June 22, 1996, 55 residents (and several developers) met at Slater School to generate a long list of ideas on how to improve their neighborhood. The participants met in small groups with Planning Commissioners and City staff where they shared their ideas about the neighborhood using large maps. After brainstorming ideas and then voting on top issues, each group presented a summary to the whole workshop. The key suggestions

included: redesign Whisman Road with landscaped medians, improve streets, deal with traffic in specific locations, improve bike and pedestrian conditions, make better use of the Hetch-Hetchy right-of-way, upgrade shopping areas, do something about the Fairchild-Evandale area, beautify the neighborhood, make public safety a priority and ensure high-quality industrial development. Workshop participants signed up to be members of focus, or interest, groups for several of the issues like the Fairchild-Evandale area, traffic, shopping and open space.



The workshop and planning kit suggestions shaped the planning process that followed. A traffic study and a neighborhood shopping study were commissioned. Staff prepared background reports on population, the Superfund site and hazardous materials use in the industrial area. Starting in January 1997, the Commission discussed at least one

aspect of the Whisman Area at almost every meeting through July (14 meetings altogether). Staff reports were sent to focus group members, and residents and property owners came to the meetings to present their views. Two additional neighborhood meetings were held: In November 1996, a meeting was held with residents and property owners in the Fairchild-Evendale neighborhood to get input on possible revisions to the Precise Plans for this area. In April, a meeting was held at Slater School to discuss the traffic study and landscaped medians. Also, the City's Bicycle/Pedestrian Advisory Committee reviewed the pedestrian circulation plan in March, and the Parks and Recreation Commission reviewed the Streetscape Guidelines and the Parks and Open Space Element in June.

Throughout the planning process, representatives of the Middlefield-Whisman Neighborhood Association have provided staff with suggestions and guidance on how to notify and involve residents in the process.

The culmination of the public participation process is this Whisman Area Plan, which the City Council approved on December 9, 1997.

Environmental Review

An Initial Study of environmental impacts and a Mitigated Negative Declaration were approved for the Whisman Area Plan. The major environmental issues related to development in the industrial area are traffic and the increased demand for housing. The major issues related to residential development in the Evendale Precise Plan area are noise, hazardous materials and possible displacement of low- and moderate-income residents. Mitigation measures were identified for all of the impacts. The mitigation measures include traffic signals and pedestrian improvements. They also include design measures that have been incorporated into the Precise Plan, such as a density bonus for low- and moderate-income housing, requirements for noise studies, and hazardous materials use restrictions. The Mitigated Negative Declaration found that all potentially significant environmental impacts could be reduced to a less than significant level.

OVERVIEW OF PLAN

General Land Use Pattern

A basic planning conclusion in the Whisman Area is that the general land use pattern (see Figure 2) is acceptable and functional. The land use pattern may not be ideal, but the benefits of making significant changes would be more than offset by the disruption to residents, businesses and property owners.

The mixture of residential zoning categories will remain as it is except in the Evandale Precise Plan area, where the Fairchild Drive frontage is proposed to be changed from commercial to residential (allowing an additional 75 to 100 housing units). Even though no other zoning changes are proposed, there will continue to be change in the residential neighborhoods as vacant and underdeveloped parcels fill in (e.g., on Ada Avenue, Evandale Avenue where the zoning remains residential and Whisman Road near the Whisman Station neighborhood). These development decisions will be made by individual property owners.

The zoning in the industrial area will remain ML (Administrative, Research, Limited Industrial), although market forces are gradually changing it from manufacturing and warehouses to offices and R&D. Some older businesses such as Raytheon and Air Products are expected to remain, but tenants like Netscape and KPMG-Peat Marwick are filling the new buildings. Higher intensity development near the transit station will be considered on a case-by-case basis under the provisions of the Transit (T) zone.





The boundaries of the commercial areas will also remain the same except for change to the Fairchild Drive frontage (from Whisman Road to Leong Drive) described above. The table below summarizes land use in the Whisman Area.

SUMMARY OF EXISTING AND PLANNED LAND USE

	RESIDENTIAL	INDUSTRIAL
Existing 1990	3,312 units	2,850,400 square feet
Built 1990-96	36	15,000
Approved, Not Yet Built 1997	549	669,600
Development Potential Unrelated to Whisman Plan	130	1,149,600
Development Potential Related to Whisman Plan	100	669,400
TOTAL DEVELOPMENT CAPACITY	4,127 units	5,354,000 square feet



LEGEND:

-  Industrial
-  Residential
-  Commercial
-  Schools/Parks

Whisman Area Plan	Figure 2
Land Use	

Proposals

Following is a summary of the major sections in the Whisman Area Plan and the proposals in each.

Community Image

The intersection of Whisman and Middlefield Roads is the center of the Whisman Area. Design features are proposed to emphasize this role. An evaluation of nearby land uses is also proposed.

Residential Area

The major proposals in the residential area are:

- ◆ Several traffic improvements (traffic signals and stop signs)
- ◆ Landscaped medians on Whisman Road
- ◆ A new Evandale Precise Plan to replace two existing Precise Plans
- ◆ A community appearance strategy
- ◆ A parks and open space plan

Commercial Areas

The major proposal for the commercial areas are:

- ◆ Improving the existing neighborhood shopping areas at Middlefield and Whisman Roads and on Leong Drive
- ◆ Zoning revisions in the industrial area that will provide more employee-serving commercial uses

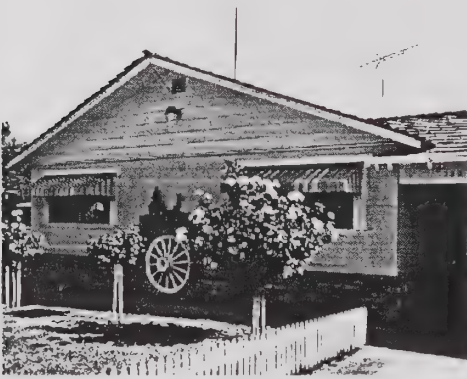
Industrial Area

The major proposals in the industrial area are:

- ◆ Streetscape Guidelines
- ◆ Several new traffic signals
- ◆ A pedestrian circulation plan in the vicinity of the Middlefield Light Rail Station
- ◆ Middlefield Road landscaped median

COMMUNITY IMAGE

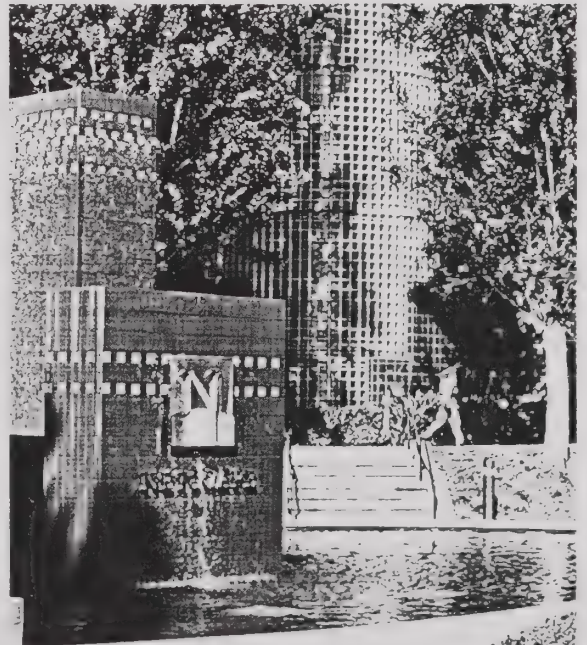
The Whisman Area has had no clear landmark or central focal point. Some of the large old industrial plants, like Fairchild and GTE, have served that purpose by virtue of occupying large areas of land, but these companies have moved out or have scaled back. At the neighborhood level, the rustic wagon wheel, built into the front facades of scattered single-family houses and duplexes, is a symbol for the area. This same wagon wheel is the namesake for the restaurant on Middlefield Road where scientists and entrepreneurs met in an earlier day to lay out their plans and dreams for what would become Silicon Valley.



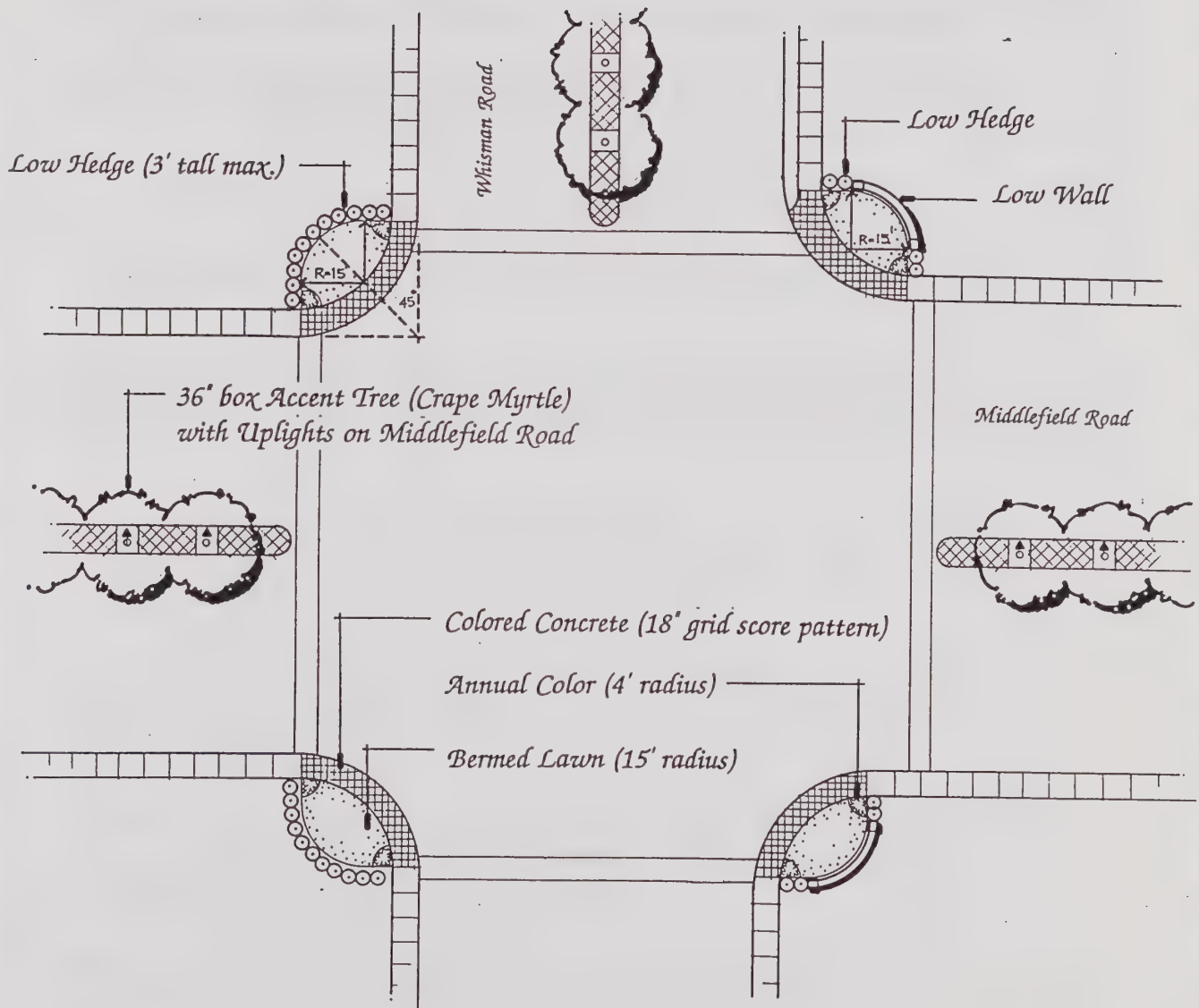
Wagon wheels decorate the facades of houses scattered through the area.

The intersection of Whisman and Middlefield Roads is the ideal location to express this new image through landmark features. It is the geographic center of the community—the point where the residential, commercial and industrial areas meet. New landscaped medians are proposed for two of the four roads leading to this point, and a landscaped median on a third road (Middlefield in the residential area) was recently replanted and upgraded. These medians link the community together and will culminate in special design features at the intersection of Middlefield and Whisman as shown in the the Figure 3 illustration.

As the industrial property in the Whisman Area redevelops, it will begin to give the area a new high-tech/office park image. Corresponding changes are needed in the public areas to strengthen and reinforce this new image. Converging with this objective, Whisman residents have asked for better public landscaping and open space. Together, the public and private improvements will help demonstrate that the Whisman Area is a distinct and identifiable area of Mountain View.



Netscape's expansion plans are helping to change the image of the industrial area.



Whisman Area Plan

Middlefield-Whisman Focal Point

Figure

3

Each of the four corners of this intersection would have a gently bermed lawn area with a convex back border and flowering perennials. Each corner would also have similar textured and colored sidewalk pavement. Crape Myrtle trees would be planted in the nose of the Middlefield medians on both side of Whisman Road. These improvements should be implemented by the City through a Capital Improvement Program, with ongoing maintenance the responsibility of the property owner (or lessee). Two additional elements are proposed for further study: entry signs to identify the Middlefield-Whisman neighborhood and the Ellis-Middlefield industrial district, and undergrounding the overhead utilities crossing the intersection.

Another proposal for further study is:

- ◆ **Evaluate the mix of land uses in the larger area around the Middlefield-Whisman intersection.**

The purpose of this proposal is to evaluate whether there are other ways of better integrating the geographically separated land uses surrounding the center of the community.

THE MIDDLEFIELD-WHISMAN RESIDENTIAL AREA

Population, Housing and Schools

The 1990 Census contained the following information about the people who live in the Middlefield-Whisman residential area.² This area:

- ◆ Had a population of 7,430 people and 3,312 housing units in 1990.
- ◆ Was one of the faster growing areas of the City between 1980 and 1990 with an 18 percent increase in population (compared to 12 percent for the City as a whole) and an 11 percent increase in housing units (compared to 8 percent for the City as a whole).
- ◆ Almost doubled its supply of single-family units between 1980 and 1990, with much of that growth in townhouses (which are included as "single-family attached" in the Census).
- ◆ Has a higher percentage of townhouses (27 percent) than any other area of the City, but a low percentage of traditional single-family houses (12 percent) compared to most other areas.
- ◆ Reflected the Citywide average of one-third ownership and two-thirds rental units. A higher percentage of single-family units were rented (38 percent) as compared to 28 percent for the City as a whole.
- ◆ Was one of the newer areas of town with about 50 percent of its housing units built between 1970 and 1990 compared to about 37 percent for the City as a whole.
- ◆ Is somewhat more ethnically diverse than the City as a whole.
- ◆ Had a somewhat younger population than the City as a whole with a greater proportion of young children and adults under age 45.

²Background Report, Population Data for the Whisman-Middlefield Area," June 6, 1996.



Whisman Area Plan	Figure
Residential Area	4

- ◆ Had a median household income of about \$43,500, which was about \$5,000 less than the median for the City.
- ◆ Has a higher percentage of single-parent families than other area of the City (36 percent as compared to 27 percent).

The mix of single-family houses, apartments, condominiums and townhouses is very similar north and south of Middlefield Road, but there are many more duplexes north of Middlefield.

Since 1990, 36 new housing units have been built in the Whisman Area and 550 new units have been approved, but are not yet completed. Most of those are at the new Whisman Station development on the former GTE site. The Whisman Station neighborhood is a mixture of townhouses and single-family houses on small lots. The community is being built under the zoning and design standards of the Whisman Precise Plan. This is the only residential development on the east side of Whisman Road, and provisions in the Whisman Area Plan are aimed at providing stronger linkages between it and the neighborhood on the west side of Whisman Road.



*Townhouses are found throughout the neighborhood.
This one is on Ada Avenue.*

Most students in the Middlefield-Whisman neighborhood attend Slater School, which is on Gladys Avenue located south of Middlefield Road. Slater is in the Mountain View School District. Students who live west of Easy Street and north of Middlefield attend Whisman School, which is in the Whisman School District and is located on Easy Street. Most of the students at Whisman School come from the area to the west of State Route 85 and from Moffett Field housing.

Goals for the Middlefield-Whisman Residential Area

The Whisman Area Plan promotes the following goals for the residential area:

- ◆ **Strengthen and unify neighborhood identity.**
- ◆ **Preserve and enhance the diverse mix of housing types and housing prices as vacant lots are filled in and redevelopment takes place.**

- ◆ Ensure that traffic controls adequately protect the neighborhood.
- ◆ Improve the transition from the residential to the industrial side of Whisman Road.
- ◆ Clarify the desired future land use for the Evandale Precise Plan area.
- ◆ Expand park opportunities.

Traffic Improvements

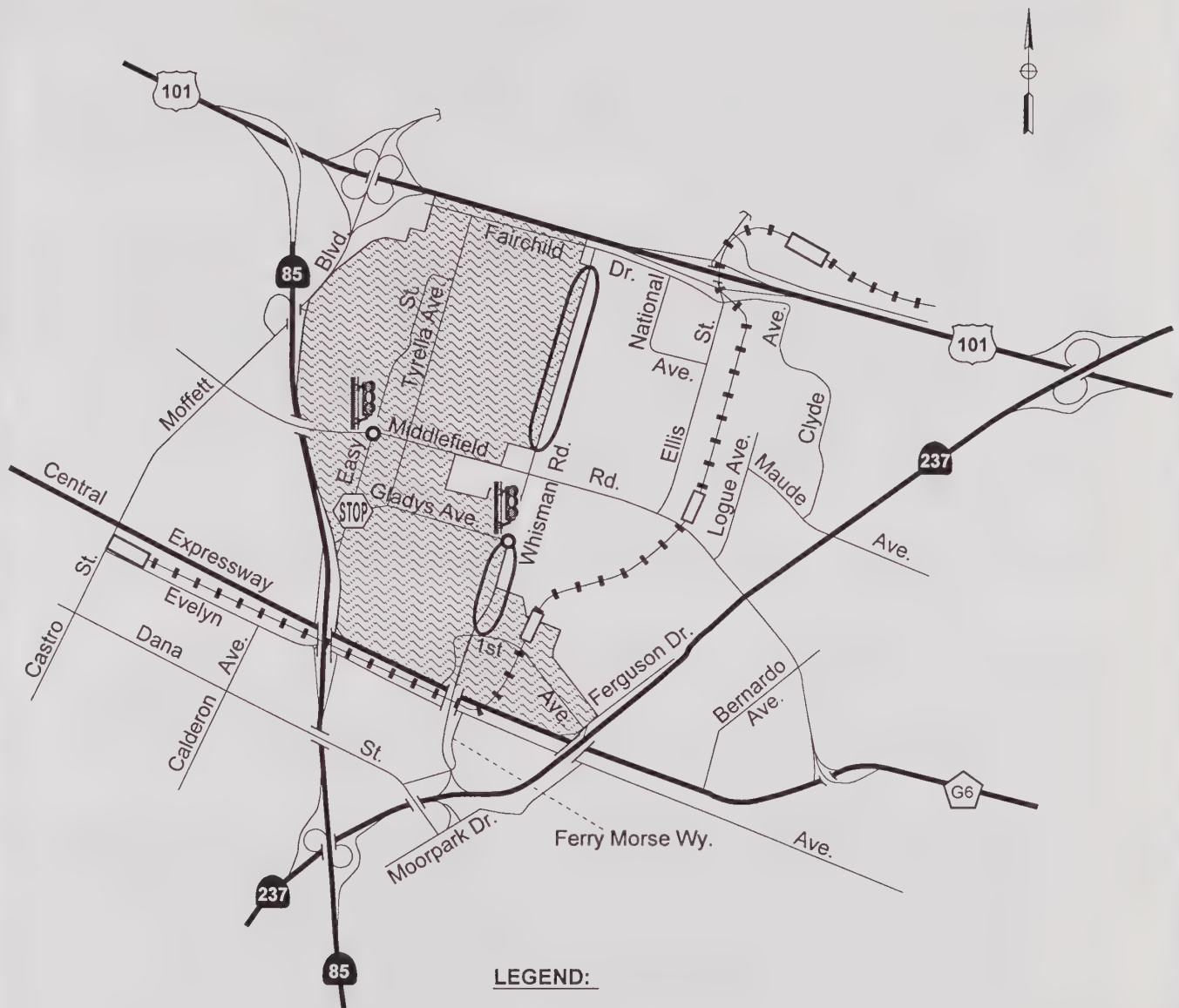
Traffic was a major concern raised at the neighborhood workshop. The Whisman Traffic Study, prepared by a traffic consulting firm, analyzed all of the problem locations listed by residents and made recommendations on whether and how to respond to them.³ New traffic controls can only be installed if they meet the City's basic criteria (called warrants). These warrants consider traffic volumes, accident history and whether the stop sign or signal will have unintended impacts such as creating new accident locations and more traffic noise for those who live nearby.

Based on the Commission's review of the consultant study, the following near-term traffic improvements are recommended (see Figure 5):

- ◆ *Stop signs on Easy Street at Gladys Avenue* are to be installed when traffic volumes and increased pedestrian activity cause this location to meet the City's basic traffic control criteria. Monitoring and analysis is to occur on a quarterly basis after the Easy Street mini-park is completed. In addition, the City is to review the feasibility of reconfiguring the streets to narrow the intersection and thereby slow traffic in the area.
- ◆ *A traffic signal at Whisman Road and Gladys Avenue* (which was completed in fall 1997).
- ◆ *A traffic signal at Middlefield Road and Easy Street* is to be installed when the location meets the minimum volume and other traffic requirements for new signals.

In addition to these stop signs and signals, the proposed landscaped medians described in the following section will slow traffic on Whisman Road.

³"Whisman Traffic Study," CCS, Inc., February 28, 1997.



Whisman Area Plan	Figure
Traffic and Road Improvements in Residential Area	5

Although the other locations cited by the neighborhood do not currently meet the basic criteria for installing stop signs or signals, residents can continue to monitor these locations and bring them to the City's attention if conditions change. The City's Neighborhood Traffic Management Program describes the procedure by which residents can participate in deciding where to locate traffic controls while also ensuring consistent and cost-effective solutions.

Some of the traffic problems concerned streets that were narrow or were missing sidewalks. Ada Avenue and Minaret Avenue were cited. Normally, streets are widened and sidewalks are installed by the adjacent property owners at the time they develop their property. On both of these streets, there are vacant or underdeveloped properties, so the improvements have not been made. Minaret Avenue is a particular problem because it is quite narrow and connects Ada Avenue with Whisman Road. The cost to the City of bringing this street up to standard would be about \$750,000, which would be a significant capital improvement project.

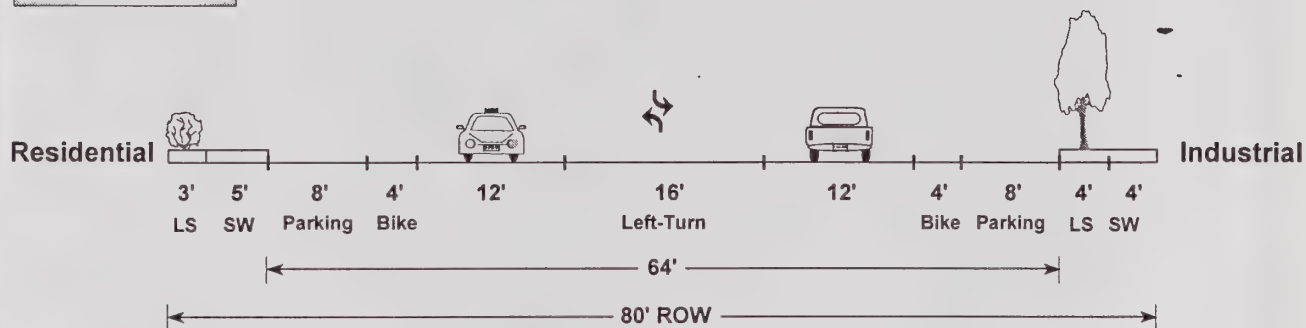
Traffic improvements in the industrial area are discussed on Pages 25 and 26.

Landscaped Medians on Whisman Road

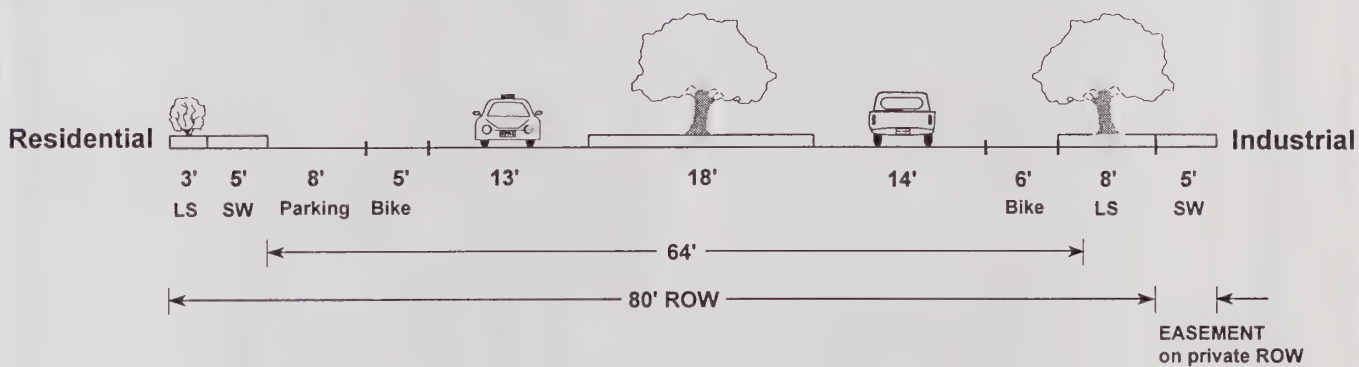
Landscaped medians are recommended for Whisman Road to provide a smoother visual and pedestrian transition between the two sides of the street.

- ◆ The *median north of Middlefield Road* will buffer the residential side of the street from industrial activities on the other side and give the street a new, softer look. The Whisman Traffic Study confirmed that future traffic volumes could be accommodated with a two-lane street. The median would be 18 feet to 20 feet wide with breaks at all residential street intersections and at major industrial driveways. Bike lanes would be retained, and there would continue to be parking on the residential side, but not the industrial side of the street. The median would be landscaped with gentle mounding and evergreen trees consistent with the Ellis-Middlefield Streetscape Guidelines (see Page 26). The median and a crosswalk will provide a safer environment for pedestrians crossing from one side of the street to the other at the Hetch-Hetchy trail proposed in the Parks and Open Space section of this Plan. Figure 6 shows street cross-sections as the street exists now and how it would look after the median is installed. Figure 7 shows locations of possible median breaks. Both figures are conceptual. Installation of this median would implement a General Plan policy that designates this segment of Whisman Road as a Residential Arterial.

EXISTING



PROPOSED



LEGEND:

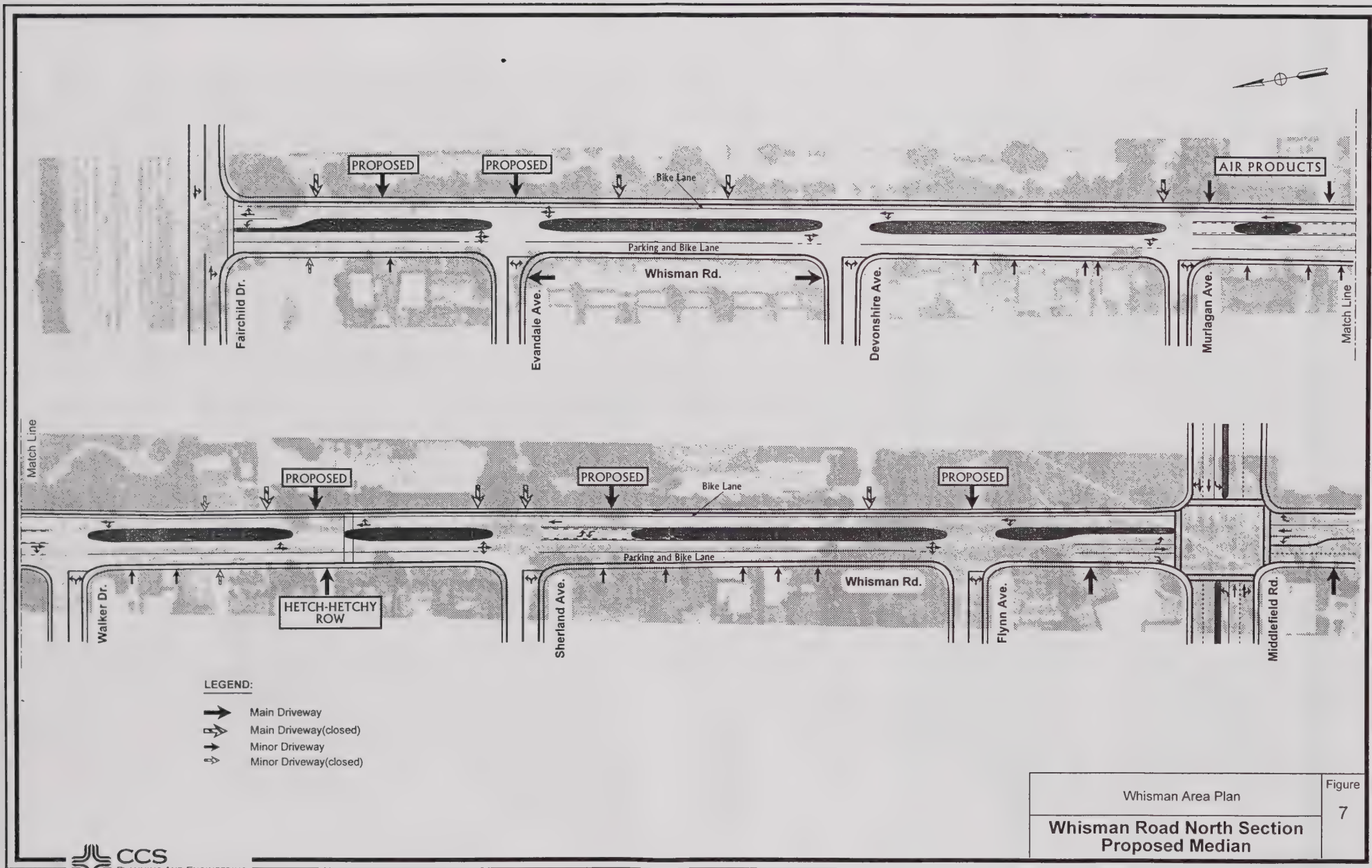
ROW Right-of-way
 SW Sidewalk
 LS Landscape Strip

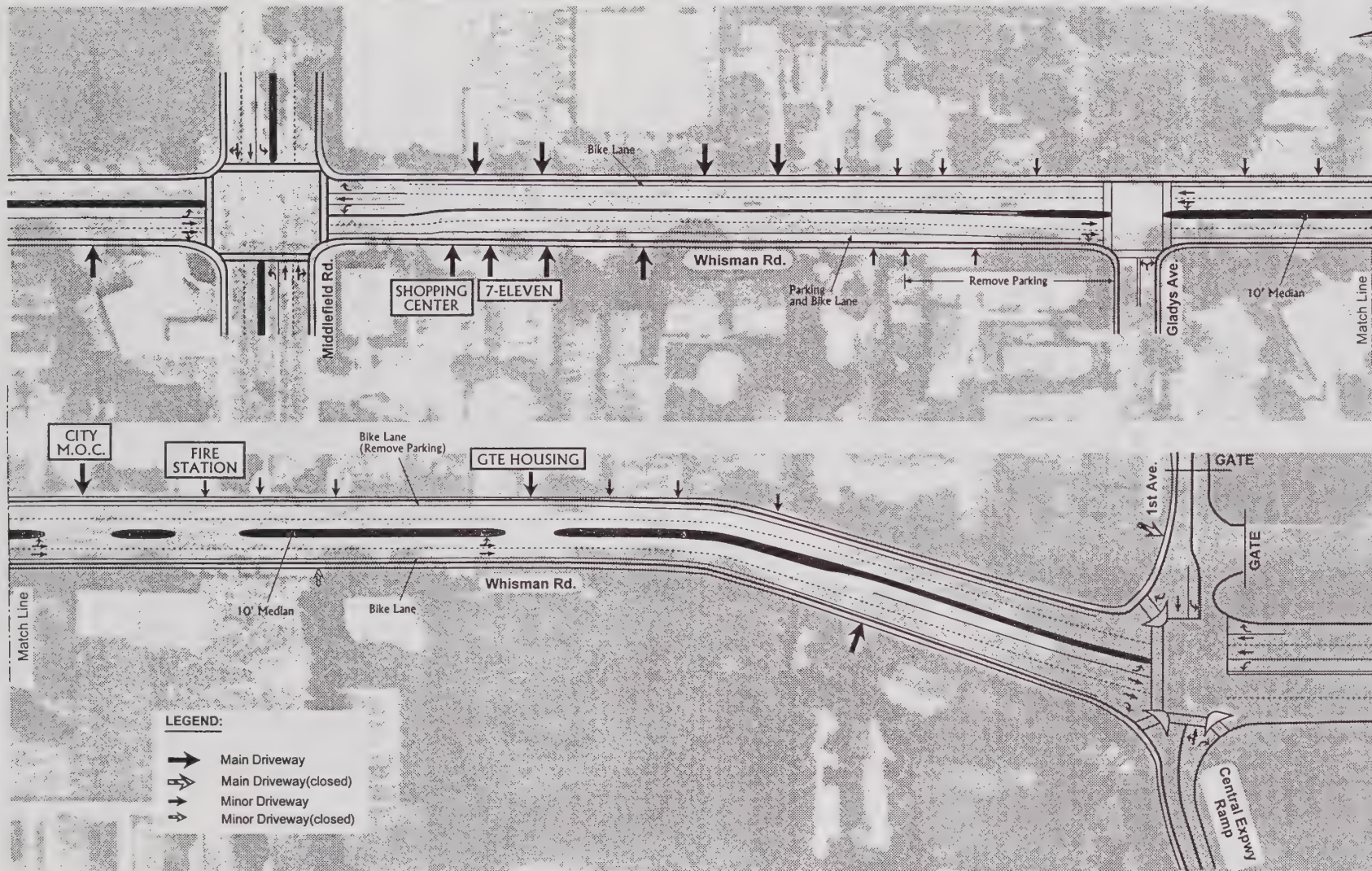
Whisman Area Plan

**Whisman Road North
 Cross-Sections**

Figure

6





Whisman Area Plan	Figure
Whisman Road - South Section Proposed Median	8

- ◆ The *median south of Middlefield Road* would be narrower (6 feet to 10 feet wide) because the street must continue to have four lanes to accommodate future traffic volumes. This segment of Whisman carries more traffic because it provides direct access to Central Expressway, Route 237 and other parts of Mountain View. The median would begin about 200 feet north of Gladys Avenue, break at Gladys Avenue, then continue south to the signalized intersection at the Whisman Station entry (with additional breaks at several intersections and driveways.) Figure 8 shows possible median breaks. The street would have bicycle lanes, but parking, which is currently limited, would be further reduced by removing it along the length of the median. A small section of parking would remain next to residential development on the west side. Since the purpose of this median is to tie together the two residential sides of the street and provide a safe pedestrian crossing at Gladys, the median would be flat with a more open planting scheme and canopy trees.

The median designs are conceptual. Detailed design would occur when the project has been funded through the City's Capital Improvement Program estimated for Fiscal Year 2001-2002.

Evandale Precise Plan

A new Precise Plan was approved for the Fairchild-Evandale-Leong Area with the objectives of bringing about residential redevelopment on Fairchild Drive and strengthening the neighborhood commercial center. See Evandale Precise Plan (Attachment A).

The Evandale Precise Plan covers a one-block wide strip of land adjacent to U.S. 101 between Moffett Boulevard and Whisman Road. (See Figure 9.) It combines two pre-existing Precise Plans: the Fairchild-Evandale Precise Plan and the Leong-Fairchild Area Precise Plan. The new Precise Plan also includes some land on the west side of Leong Drive that is currently within the P Zone, but without a Precise Plan. Much of the area was developed in the 1950s and 1960s, although some commercial development dates back to the 1940s. The commercial uses on Fairchild Drive are outdated and no longer have freeway visibility because of a sound wall. The Precise Plan changes the Fairchild Drive frontage from commercial to residential.



Whisman Area Plan	Figure
Evandale Precise Plan Area	9

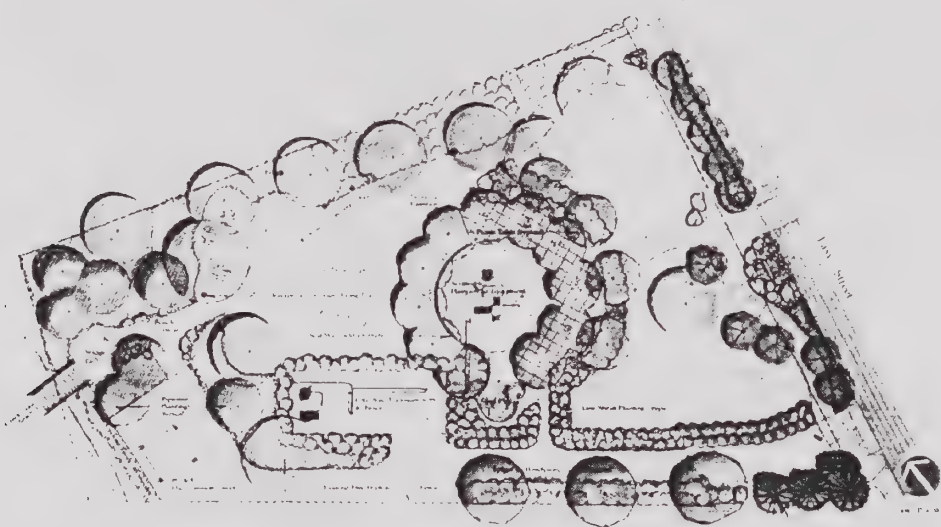
The Precise Plan is divided into Areas A, B and C.

- ◆ *Area A is commercial.* It encompasses the Leong-Fairchild Area Precise Plan and the three commercial businesses across Leong Drive. This area provides neighborhood shopping for the northerly half of the Whisman neighborhood as well as some freeway-oriented commercial services. In this area:
 - * Neighborhood commercial uses and hotels or motels are the permitted uses.
 - * New parking and sign requirements offer opportunities to create a more coordinated and unified shopping area to serve the larger Whisman neighborhood.
- ◆ *Area B is residential.* It encompasses the Fairchild-Evandale Precise Plan. In this area:
 - * Multiple-family residential development is the principally permitted use throughout the area.
 - * Existing commercial uses become nonconforming.
 - * Basic residential densities range from 20 to 25 units per acre for smaller parcels to 26 to 30 units per acre for parcels greater than 2.5 acres. Density bonuses of approximately 25 percent are allowed for developments that provide at least 20 percent lower-income units or 10 percent very low income units.
 - * Hotel and motel use is allowed on parcels adjacent to Area C.
 - * Freeway noise is a concern in this area and noise attenuation measures are required.
- ◆ *Area C is offices.* It is the half-acre parcel at the corner of Whisman Road and Fairchild Drive.
 - * Office and research and development use is allowed where a new building was built in 1997.
- ◆ *Nonconforming uses* must terminate in 20 years.

The Precise Plan was adopted concurrently with the Whisman Area Plan, as were related General Plan land use changes.

Parks and Open Space Element

The Whisman Area has two existing open space areas, totaling 12.8 acres. They are located at the schools (Whisman is a joint City/school park). In addition, the Stevens Creek Trail, a major new north-south bicycle and pedestrian corridor, parallels the western boundary of the area. In 1997, three mini-parks were nearing construction: a mini-park on a City well site near the intersection of Easy Street and Gladys Avenue and two mini-parks in the Whisman Station (GTE) residential neighborhood. Even with all of these open space areas, the Parks and Recreation Commission has determined that the Whisman Area has less than the recommended 3 acres of open space per 1,000 people.



Plans for the new mini-park on Easy Street, near Gladys Avenue.

The Parks and Open Space Element (Attachment B) contains three proposals (see Figure 10):

- ◆ Increase the amount of park land serving neighborhood residents.
 - * Find a site for a mini-park in the *Tyrella/Evandale* neighborhood. This is an area which has been determined to be particularly deficient in parks. (A park site on Devonshire was purchased in early 1998.)
 - * Consider other possible open space opportunities such as the *orchard property* next to the City's Municipal Operations Center on Whisman Road. This site may be appropriate for a community orchard and garden and other uses that commemorate the area's agricultural history.
- ◆ Develop a trail, and possibly a park with community gardens, on the *Hetch-Hetchy right-of-way* where it passes through the Whisman neighborhood. Although the Hetch-Hetchy right-of-way is 80 feet wide in most places, it widens



Whisman Area Plan

Parks, Open Space and
Trail Plan

Figure

10

out to 250 feet in the block between Whisman and Tyrella Avenue creating a six-acre open space area. In 1997, the Hetch-Hetchy right-of-way is leased to a nursery for the growing and storage of plants in containers. It would be possible for a trail and the nursery to share the space. In the future, it may be possible to use the entire site for a park.

- ◆ Create a *comprehensive system of bicycle and pedestrian paths* connecting the industrial and residential areas with one another and with other parts of the City. This system would tie together three off-road trails—the Stevens Creek Trail, the proposed Hetch-Hetchy trail and the planned Light Rail trail, as well as the secondary pedestrian connections within the industrial area (see the Pedestrian Circulation Plan on Page 29). The proposed landscaped medians on Whisman Road also contribute to the system by providing a “safe haven” for pedestrians who are trying to cross these wide streets.

If all of these proposals are implemented, the amount of park and open space land in the Whisman Area (not including the Stevens Creek Trail) would be 16.8 acres to 22.8 acres, with the amount depending on whether the Hetch Hetchy right-of-way is developed into a park. This comes out to 2.0 acres to 2.7 acres per 1,000 people.

Community Appearance

Community appearance and possible City code violations have been a concern of neighborhood residents. The City has a Neighborhood Preservation Strategy, the goal of which is to work with neighborhoods to identify and implement solutions to these problems. The Whisman Area Plan lays out objectives and actions specific to this area. The recommended objectives are:

- ◆ Encourage interest and involvement from Whisman area residents in their neighborhood.
- ◆ Increase the visibility and effectiveness of City code enforcement programs in this area.
- ◆ Target major blighting influences in the Whisman Area for concentrated City code enforcement efforts.

The plan for this area, “Improving Community Appearance in the Whisman Area,” is Attachment C. The actions include working with neighborhood associations and residents who want to band together to address problems, enforcing the Rental Inspection Program, making at least annual field checks of the Evandale/Fairchild area to get rid of major blighting problems like abandoned cars, weeds and debris, community-oriented policing actions, responding to general code complaints, and introducing special initiatives such as neighborhood cleanups where there is demonstrated local interest.

COMMERCIAL AREAS

Two neighborhood shopping areas are located in the Whisman Area. One is at the corner of Whisman and Middlefield Roads and one is on Leong Drive near the freeway (see Figure 2).

Middlefield-Whisman Corners

This is the major center of commercial activity in the Whisman Area. On the south side of Middlefield, Sun-West Plaza and four other properties contain a total of 53,000 square feet of commercial floor area. Sun-West has a mix of retail, food and personal services, a child care center and a karate studio. The four other properties are a fast food restaurant, a delicatessen/donut shop, a 7-Eleven store and a fitness center. Since there are five separate properties, each with its own parking, the shopping area does not function as a unified center. Poorly distributed parking has been a problem, and in the past, there have been vacancies at Sun-West.



The commercial center at the Middlefield Road/Whisman Road corner must fulfill diverse community shopping needs.

On the north side of Middlefield, there is a gas station and a restaurant (the Wagon Wheel) which together provide an additional 6,800 square feet of commercial floor space. Only a very small portion of the parcel on which the Wagon Wheel is located is zoned commercial (a lot area of about 10,000 square feet). The rest of the parcel is zoned for residential uses. If the Wagon Wheel site is redeveloped, most of the Middlefield Road frontage would become residential, reducing the commercial area.

Leong Drive

The Leong Drive area is within the new Evandale Precise Plan area. There are about 26,400 square feet of retail and restaurant uses in this area, plus two motels. The businesses include a grocery store/delicatessen, two restaurants, a bar, bar supply business and a cleaners. The buildings on the easterly side of Leong Drive were built in the 1950s and had historically served clientele from Moffett Field, which is directly across U.S. 101. Some of the businesses are marginal now that the military presence at

Moffett Field has been reduced. The motel, restaurant and gas station on the westerly side of Leong Drive were built in the 1970s and are stable.

Industrial Area

Two restaurants are located within the industrial area—an older business at the corner of Fairchild Drive and Ellis Street and a small restaurant at the rear of an industrial building.

Commercial Needs

There is strong resident interest in improving quality and services at the two neighborhood shopping areas—at Middlefield/Whisman and Leong Drive. An economic analysis was commissioned to find out whether the Whisman Area could attract a supermarket if land was zoned for that purpose. A consulting firm that advises food stores analyzed the current and projected population of the “trade area,” including employees in the industrial area, their buying power and the strength of food store competition.⁴

The study indicated that the Whisman trade area is already adequately served by three existing major supermarkets (Safeway, Lucky, Nob Hill) even though none of the stores is actually located in the area. Although the study concluded that a supermarket was not feasible, it did support food store expansion. The consultant recommended a store of 5,000 to 7,000 square feet. The results of the food store study were shared with the commercial property owners in the area.

Employees in the industrial area also need nearby eating places and some personal and retail services. This will be especially important for employees who will commute by train and will not have their cars during the day to go out for lunch or do errands. Some large employers provide cafeterias and about one-fourth of the industrial area is within walking distance (2,000 feet) of the Middlefield-Whisman commercial area. Also, the Light Rail will be available to take employees to Downtown Mountain View where there are many eating choices for longer lunch hours. However, there is still a recognized need for additional commercial services.

⁴“Neighborhood Shopping Analysis for the Whisman Area,” Keyser-Marston Associates, Inc., July 1996.

Goals for the Commercial Areas

The Whisman Area Plan promotes the following goals for the commercial areas:

- ◆ **Improve the existing neighborhood shopping areas at Middlefield and Whisman and on Leong Drive.**
- ◆ **Provide more employee-serving commercial uses.**

Proposals to Increase Commercial Services

The following strategies are recommended:

- ◆ Clarify the role of the *Whisman and Middlefield Commercial Area* in serving the area by adopting the following policy statement:

The commercial area at Middlefield and Whisman Roads should serve both the employees in the industrial district and the residents of the neighborhood, while ensuring that parking and circulation are adequate and the center is well maintained.

- ◆ Protect the size of the existing commercial area at Middlefield and Whisman Roads by considering the establishment of commercial zoning on the portion of the *Wagon Wheel* parcel that fronts on Middlefield Road and perhaps the adjacent small residential parcel. This would occur at the time redevelopment is proposed.
- ◆ Amend the ML Zone to allow *employee-serving commercial uses as a permitted use in industrial buildings* subject to a 3,000 square foot size limitation, the provision of adequate parking and location criteria that would consider proximity to extremely hazardous materials. The proposed amendment is attached (Attachment D). The ML Zone amendment was adopted at the same time as Whisman Area Plan.
- ◆ Modify the zoning regulations in the *Leong Drive* area to create a more unified and coordinated shopping area. The Evandale Precise Plan does so by providing for a unified sign program and shared parking.
- ◆ Have the City, through its *economic development* function, take a proactive role in assisting in bringing quality commercial development to the Whisman Area.

ELLIS-MIDDLEFIELD INDUSTRIAL AREA

History and Current Trends

The Ellis-Middlefield industrial area had 2.5 million square feet of industrial floor area, with another 1 million square feet under construction in 1997. There are about 30 acres of vacant land out of a total of 300 acres.

GTE and Raytheon were the first businesses to move into the area, starting in the 1950s. Fairchild Semiconductor developed most of the land along Whisman Road in the 1960s, while National Avenue, Maude Avenue and the north side of Middlefield Road were subdivided and developed about the same time. Industrial development expanded into the Clyde Avenue area and along the south side of Middlefield Road in the 1970s. From 1980 to 1997, the only new buildings constructed in the area were two buildings now occupied by Netscape on the south side of Middlefield at Ellis Street.

In the early 1990s, two major transportation projects were approved. One is a major interchange at Route 237 and Middlefield Road (which was completed in 1997). The interchange raised Route 237 to freeway status and removed a major congestion point. The other major transportation project is Light Rail (see Page 22).

The renaissance of the Ellis-Middlefield area began in earnest in 1996 with developers submitting plans for major buildings and remodeling projects. These buildings have a decidedly different character and use than the 1950s and 1960s one-story tilt-ups. They will house office workers and researchers rather than blue collar workers on assembly lines.



Whisman Area Plan	Figure
Industrial Area	11

Goals for the Ellis-Middlefield Industrial Area

- ◆ Improve development standards, streetscape guidelines, pedestrian circulation, traffic controls and other public facilities to accommodate a new wave of development in a transit-oriented setting.
- ◆ Create a more attractive and cohesive identity for the area reflecting its transition to a first-quality industrial and high-technology research park.
- ◆ Ensure that the street system has adequate capacity to accommodate the desired levels of industrial growth without adversely impacting residential areas.
- ◆ Upgrade and improve the pedestrian and bicycle circulation system, especially in the vicinity of the Light Rail stations.
- ◆ Improve the design and appearance of the major streets with ample landscaping, especially at major entryways and other focal points and between the residential and industrial areas.
- ◆ Make it easier for restaurants and other businesses that serve employees to locate in the industrial area.

The Tasman Light Rail Line

In 1992, the Board of the Santa Clara Valley Transportation Authority (VTA) chose Mountain View for the next extension of its light rail system. The Tasman line will begin in North San Jose, where it will connect to the north end of the 21-mile Guadalupe line. From there, it will extend into Sunnyvale and Mountain View to the CalTrain station on Castro Street, a distance of 7.6 miles.

It will pass through the Whisman Area on an old railroad spur (or “drill track”) that historically served Moffett Field. (The track will continue to be used for occasional heavy rail service.) Two station locations were established: One about 50 feet north of Middlefield Road (the Middlefield Station) and the other about 1,200 feet north of



Central Expressway (the Whisman Station). A third station will be located just north of U.S. 101—the Bayshore/NASA Station. Construction began in 1997 and is expected to be completed in 2001.

Mountain View is committed to the success of Light Rail. It has made a \$15 million contribution, which is the “local share” needed to leverage Federal and State rail transit grants. In addition, the City has adopted transit-oriented development plans and development regulations to ensure land use that complements and encourages light rail use. These plans and policies implement a General Plan policy that says: “Make land use plans for the Light Rail corridor that will complement and enhance Light Rail usage.”

The Transit Zone

In 1995, Mountain View adopted the Transit (T) zone, which is a “combining zone” that can be applied to industrial and commercial properties that lie within 2,000 feet of a rail station. It allows a 0.5 Floor Area Ratio (FAR) instead of the 0.35 FAR generally allowed by the underlying zoning. In return, the development must meet certain transit-supportive criteria. The T Zone is part of a larger regional vision to focus growth in locations that are well-served by transit. Higher intensity near rail stations allows continued economic expansion but in ways that will result in cleaner air and less traffic

congestion than would be the case if the same amount of development were built with a freeway orientation.



Construction begins on first development project built under the T Zone. This site is next to the Middlefield Station.

The T Zone was intended for primary use near the future Middlefield Station, and so far, two properties in that area have been rezoned. As of September 1997, only one of the two properties had sought and received approval of a transit-oriented development under the T Zone.

T-zoned properties must meet certain development criteria, including: ground-level design elements that attract and support pedestrian activity, ride-share drop-off and waiting areas, preferential parking for carpools and vanpools, bicycle parking and showers, off-site improvements to facilitate pedestrian circulation in the vicinity of the transit station, participation in a commute shuttle program until the transit station is in service and a transit pass system.



Figure 12

The Zoning Administrator may approve other transit-supportive uses, facilities, services and design such as reduced parking spaces, on-site food service, special on-site employee services and facilities, walkways, pathway lighting, waiting facilities and parking for transit patrons.

Development under the T Zone is a two-step process: City Council approval of rezoning a site to the T Zone, and Council approval of the specific development project. When deliberating on the Whisman Area Plan, the City Council considered changing the T Zone to permit areas, rather than single sites, to be rezoned to the T Zone and to rezone properties within 2,000 feet of the Middlefield Light Rail Station to the T Zone. However, the Council decided against these changes, finding the current requirement for case-by-case consideration of T Zone rezoning applications to be preferable.

Streetscape Guidelines

Streetscape are those features that comprise an individual's experience in driving, walking or biking through an area. They include roads, medians, trees, sidewalks, lights, ground-cover and shrubs, and building walls and architectural features.

Streetscape features are a powerful device for creating an identity for an area and encouraging upgrades of private property. The streetscape of the Ellis-Middlefield District evolved somewhat piecemeal during earlier decades when efficient movement of cars and trucks was the primary concern. Sidewalks are narrow or, in

some places, missing entirely. Landscaping varies in quality and design. There is nothing that distinguishes the entrances to the Ellis-Middlefield area and identifies it as a special place. The building boom has offered the opportunity to confer the area with a new image and to create a more pedestrian-focused street experience.



The streetscape guidelines contains the following major elements:

- ◆ An *overall landscaping theme* for the area encompassing redwoods and other conifers, flowering accent trees and canopy trees for pedestrian walkways.
- ◆ Landscaping plans for the *medians* as described on Pages 13 and 14.

- ◆ A simple distinctive design for the two *gateways*—U.S. 101/Ellis Street and State Route 237/Middlefield Road. The design includes a Coast live oak signature tree and a low wall with a background of Chinese pistache trees.
- ◆ A special design for the *focal point* of the industrial area which is *at Ellis Street and Middlefield Road*. The design incorporates small plaza areas, seat walls, gateway signature trees, tree wells and more formal planting arrangements.
- ◆ A special design for the *focal point at Middlefield and Whisman Roads* as described on Page 9.
- ◆ Possible landscaped medians at the *north and south ends of Ellis Street*.
- ◆ Design features for *pedestrian waiting areas* around the Light Rail stations, including seating, canopy trees, special lighting and trash containers.
- ◆ *Consistent sidewalk standards* that reflect the new emphasis on pedestrian circulation. The standards call for wider sidewalks on several streets and parkway strips on Whisman Road and Ellis Street.
- ◆ “*Front door*” design standards for the rear elevations of buildings that front on the Light Rail.
- ◆ Requirements for the *maintenance* of landscaping.

Most of the Streetscape Guidelines will be implemented as redevelopment occurs, although some elements such as median landscaping would be implemented through the City’s Capital Improvement Program. Although this area is not in a redevelopment or tax increment area, the current development activity is generating substantial new revenue for the City, a portion of which can be channeled back in the form of capital improvements.

The Streetscape Guidelines (Attachment E) were adopted at the same time as the Whisman Area Plan.

Traffic Improvements

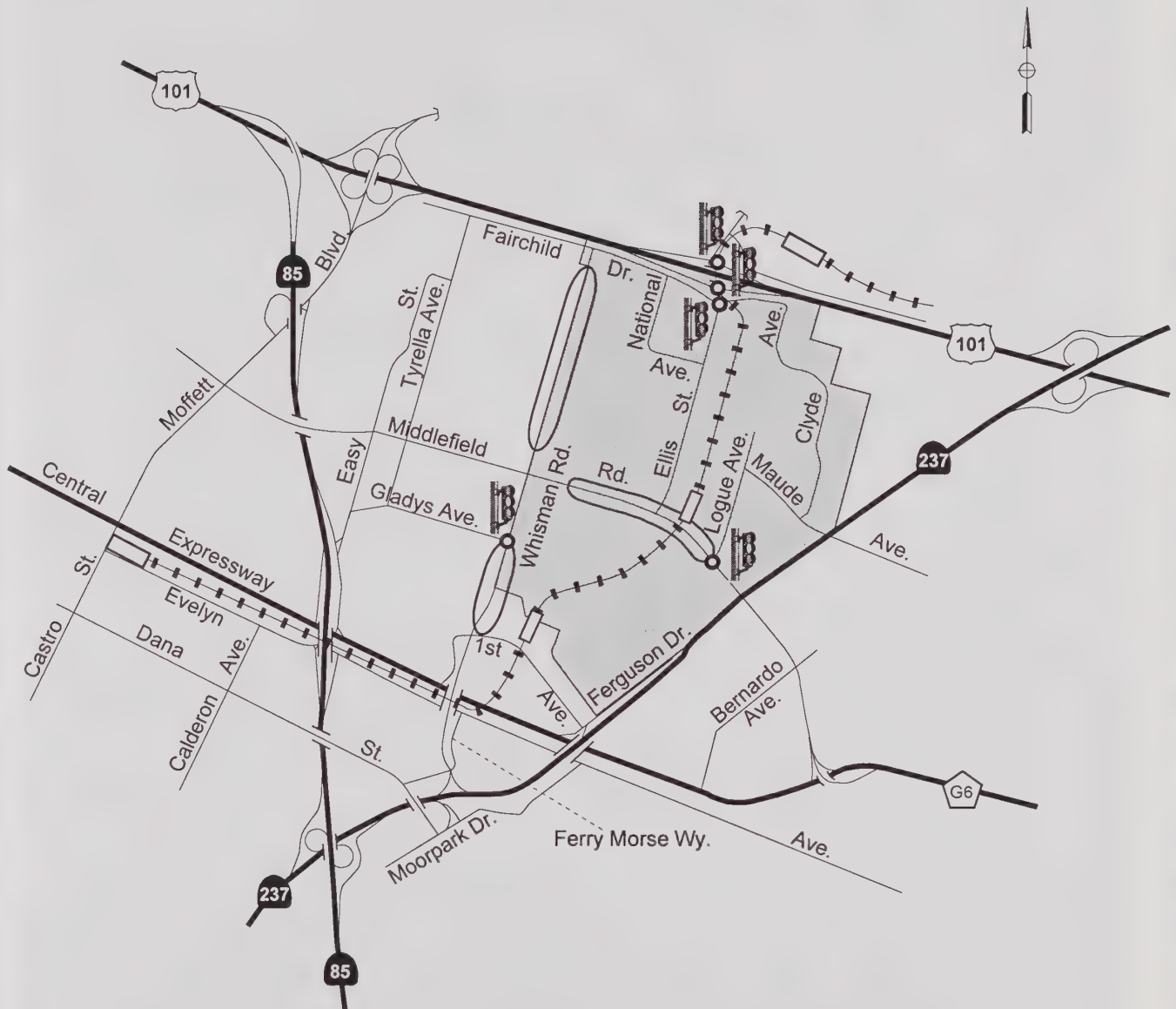
Although the Transit Zone encourages a development pattern and density that complements Light Rail, it will also result in more building area, employees and traffic. The Whisman Traffic Study analyzed the impacts of full development of the Ellis-Middlefield area if all properties within 2,000’ of the Middlefield Light Rail Station were rezoned to the T zone, plus general traffic growth in the community. The Study showed that the traffic can be accommodated on Mountain View’s internal street system provided that the traffic signals listed below and shown on Figure 13 are installed and Evelyn Avenue is widened as planned. The traffic signals are a part of the

Whisman Area Plan, although many of them have been proposed or approved in other plans. The following signals are within the industrial area and have been all or partially funded.





- ◆ *Ellis Street and U.S. 101 northbound* ramps (funded as part of the Light Rail project)
- ◆ *Ellis Street and U.S. 101 southbound* ramps (funded as part of the Light Rail project)
- ◆ *Ellis Street and Fairchild Drive* (partially funded by new development; to be installed when warranted and in coordination with U.S. 101 ramp signals)
- ◆ *Logue Avenue and Middlefield Road* (partially funded by new development; to be installed when warranted)

These traffic signals are outside the industrial area and only the first has been funded.

- ◆ *Whisman Road and Gladys Avenue* (installed fall 1997 as a GTE redevelopment mitigation measure, see Figure 5)
- ◆ *Easy Street and Middlefield Road* (to be installed when warranted, see Figure 5)
- ◆ *Ferry-Morse Way and Evelyn Avenue* (to be installed when warranted, possibly as part of Evelyn Avenue widening)
- ◆ *Calderon Avenue and Evelyn Avenue* (to be installed when warranted, possibly as part of Evelyn Avenue widening)



LEGEND:

-  Traffic Signals
-  Medians
-  Tasman LRT corridor (completed in 2001)
-  Tasman LRT station (completed in 2001)

Whisman Area Plan

Traffic and Road Improvements in Industrial Area

Figure

13

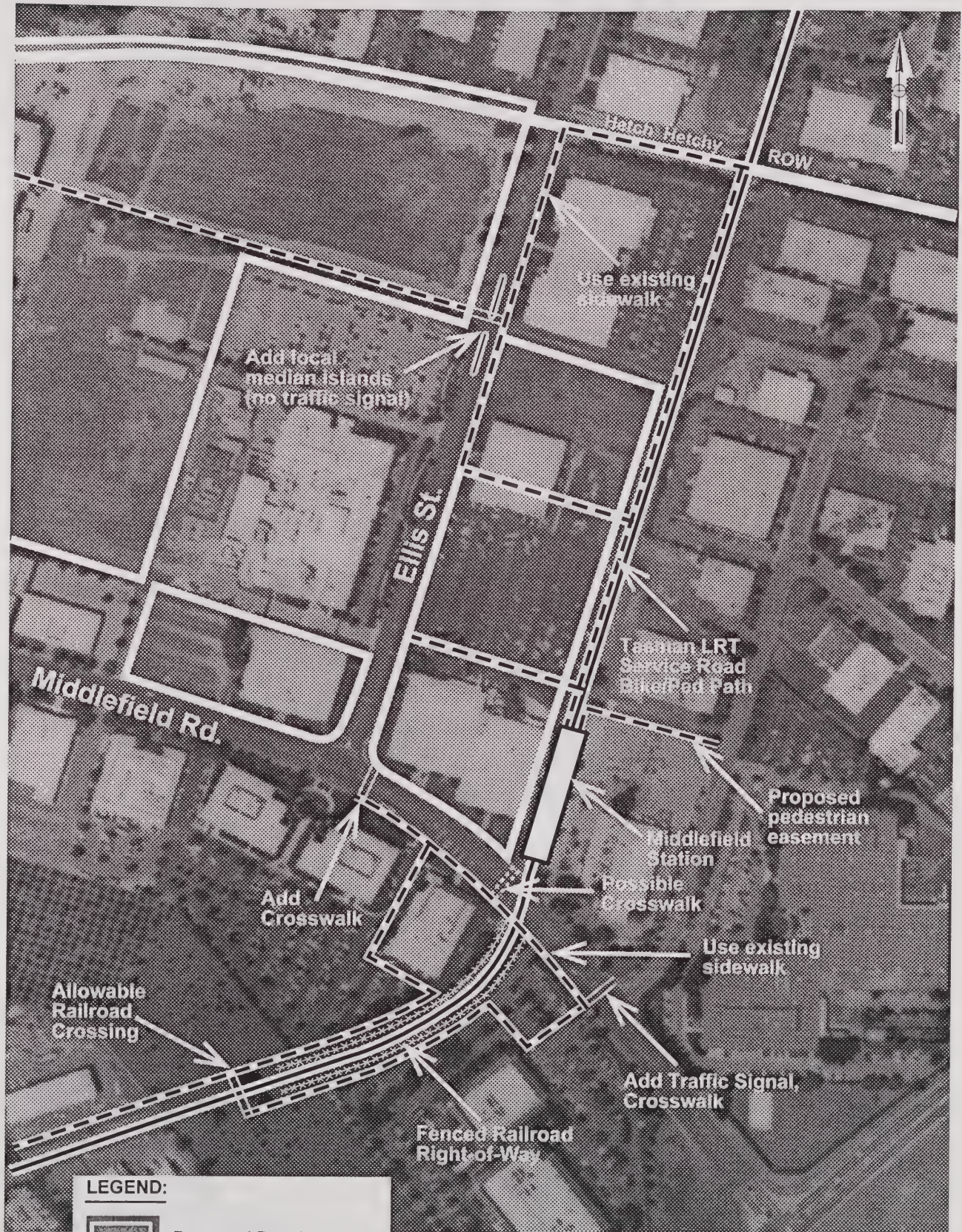
Pedestrian Circulation Plan

The advent of the Light Rail system will increase foot traffic in the vicinity of the Middlefield Light Rail Station, the Whisman Station and throughout the industrial district. To accommodate this change, a Pedestrian Circulation Plan (with many segments usable by bicyclists) is a part of the Whisman Area Plan. This system connects to the proposed Hetch-Hetchy and Stevens Creek Trails that pass through the residential area.

The Pedestrian Circulation Plan will be implemented through a combination of public projects and private development. Projects in the T Zone may have to implement portions of the plan as their share of required off-site improvements.

The major elements of the plan are shown in Figure 14 and described below:

- ◆ An 8-foot *path will parallel the Light Rail tracks* from the Whisman Station housing development on the south to the Hetch-Hetchy right-of-way on the north and across the right-of-way to Ellis Street.
- ◆ Pedestrians and bicyclists will be able to cross Middlefield Road safely at Ellis Street and Logue Avenue which have traffic signals. *An additional pedestrian signal and crosswalk* is recommended next to the tracks crossing Middlefield Road. If this mid-block crossing cannot be installed for safety and technical reasons, a barrier should be installed to prevent jaywalking.
- ◆ There will be a publicly accessible sidewalk crossing the private development project at *464 Ellis Street (Ellis Research Park)* to Whisman Road. At Whisman, there will be a crosswalk connecting the sidewalk to the proposed Hetch-Hetchy trail through the residential area. The Whisman median can be designed to facilitate pedestrian crossings.
- ◆ There will be *pedestrian connections* across private property from Ellis Street to the Middlefield Station and another connection from Logue Avenue to the station.
- ◆ The pedestrian system will be denoted with *special plantings, lighting and pavement* materials and marked with low directional signs as described in the Streetscape Guidelines.
- ◆ *Wider sidewalks* will be installed wherever it is feasible.



LEGEND:



Proposed Development



Tasman Light Rail



Pedestrian/Bicycle Path

Whisman Area Plan

Figure

14

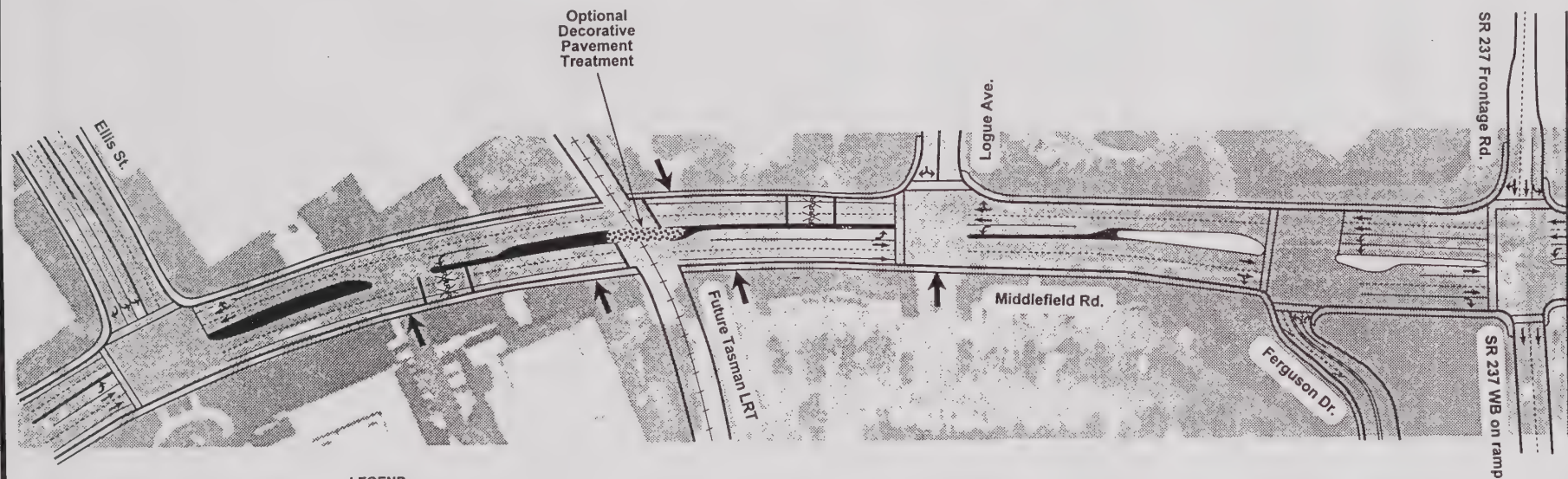
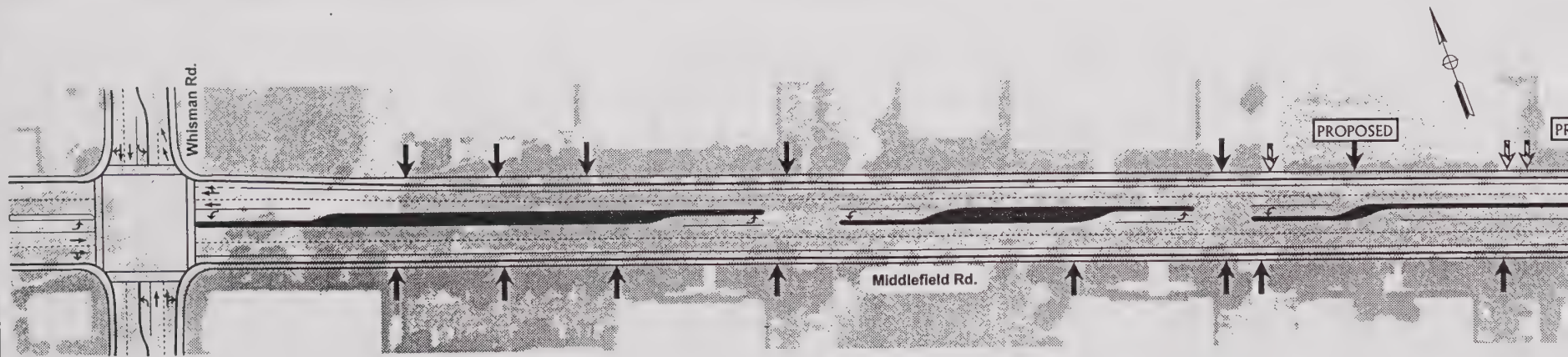
Pedestrian Circulation Plan

Middlefield Road Median




A landscaped median is recommended for Middlefield Road between Whisman Road and Route 237 connecting existing medians at either end. The roadway will have four travel lanes and bike lanes on either side. Left-turn access will be provided at intersections and major driveways. Plantings will be a continuation of those in the median further west. Figure 15 shows a conceptual design for the median. Detailed design will occur when the project has been funded through the City's Capital Improvement Program.



Another new building reaching completion on Middlefield Road.



LEGEND:

-  Main Driveway
-  Minor Driveway
-  Main Driveway (closed)

Whisman Area Plan

Proposed Middlefield Road Median

CONCLUSION

The Whisman Area Plan sets out a new course for this large and important area of Mountain View. Individually, the recommendations in the Plan may not lead to dramatic change. Together, they will have a visible and positive effect on the image of the community.

Most of the recommendations can be implemented within the next 5 to 10 years. Thus, returning to the Whisman Area in 2010, an observer will see the results of this planning effort. Instead of wide expanses of pavement and narrow sidewalks, the major streets will have a softer, greener and shadier look because of the landscaped medians and pattern of street trees. Entering the area from one of the freeways or from Middlefield or Whisman Roads, special design features will alert the visitor that he/she is arriving at a special place.

The area around the Middlefield Light Rail Station will look more urban because of the higher intensity of development. Yet, it will be alive with the flow of pedestrians traversing the office park on direct, convenient and well-lighted pathways. Vacant land on Whisman Road will have been filled with office buildings that turn a pleasant face to their residential neighbors across the street. One of the pathways crossing the industrial area will continue on to the Hetch-Hetchy trail that crosses the residential area and ties into the Stevens Creek Trail. Most of the residential area will look the same, although there will be a few new housing developments mixed in with the older ones. Most noticeably, Fairchild Drive will be transformed into a new residential neighborhood that provides a strong and firm edge to the Whisman Area's northern boundary. New mini-parks, north and south of Middlefield Road, provide nearby playgrounds and open space. Perhaps there will even be a large park on the Hetch-Hetchy right-of-way.

PROJECT PARTICIPANTS

MOUNTAIN VIEW CITY COUNCIL

Joseph S. Kleitman, Mayor
Ralph Faravelli, Vice-Mayor
Mario Ambra
Patricia Figueroa
Nancy Noe
Rosemary Stasek
Mary Lou Zoglin

ENVIRONMENTAL PLANNING COMMISSION

Patricia Showalter, Chair
D. K. Lu, Vice-Chair
Thomas Frankum
Carol Moholt
John Miller
Susan Wegner
Michael Kasperzak Jr.

CITY STAFF

Kevin C. Duggan, City Manager
Elaine Costello, Community Development Director
Michael Percy, Principal Planner
Lynnie Melena, Project Manager
Linda Lauzze, Senior Planner

CONSULTANTS

CCS, Inc.—Traffic
Keyser Marston Associates, Inc.—Neighborhood Shopping Analysis
Lauderbaugh-Hill & Associates—Streetscape Guidelines

ATTACHMENTS AND EXHIBITS

Attachment AEvandale Precise Plan
Exhibit A1Sign Program for Area A
Exhibit A2	...Excerpt—Zoning Ordinance—R3* District Regulations
Attachment BParks and Open Space Element
Attachment CCommunity Appearance
Attachment DAmendments to ML Zone
Attachment EEllis-Middlefield Streetscape Guidelines

EVANDALE PRECISE PLAN

I. PURPOSE

- A. The objective for this Precise Plan is to encourage infill development and redevelopment that integrates this area into the larger Whisman residential neighborhood. It encourages new residential development on the easterly side of the area and neighborhood commercial development on the westerly side.
- B. This Precise Plan combines two preexisting Precise Plans: the Fairchild-Evandale Precise Plan and the Leong-Fairchild Area Precise Plan, and also includes some land on the west side of Leong Drive. These three areas share common future objectives, a common geographical location and a common development history.

II. LOCATION AND DESCRIPTION OF AREAS

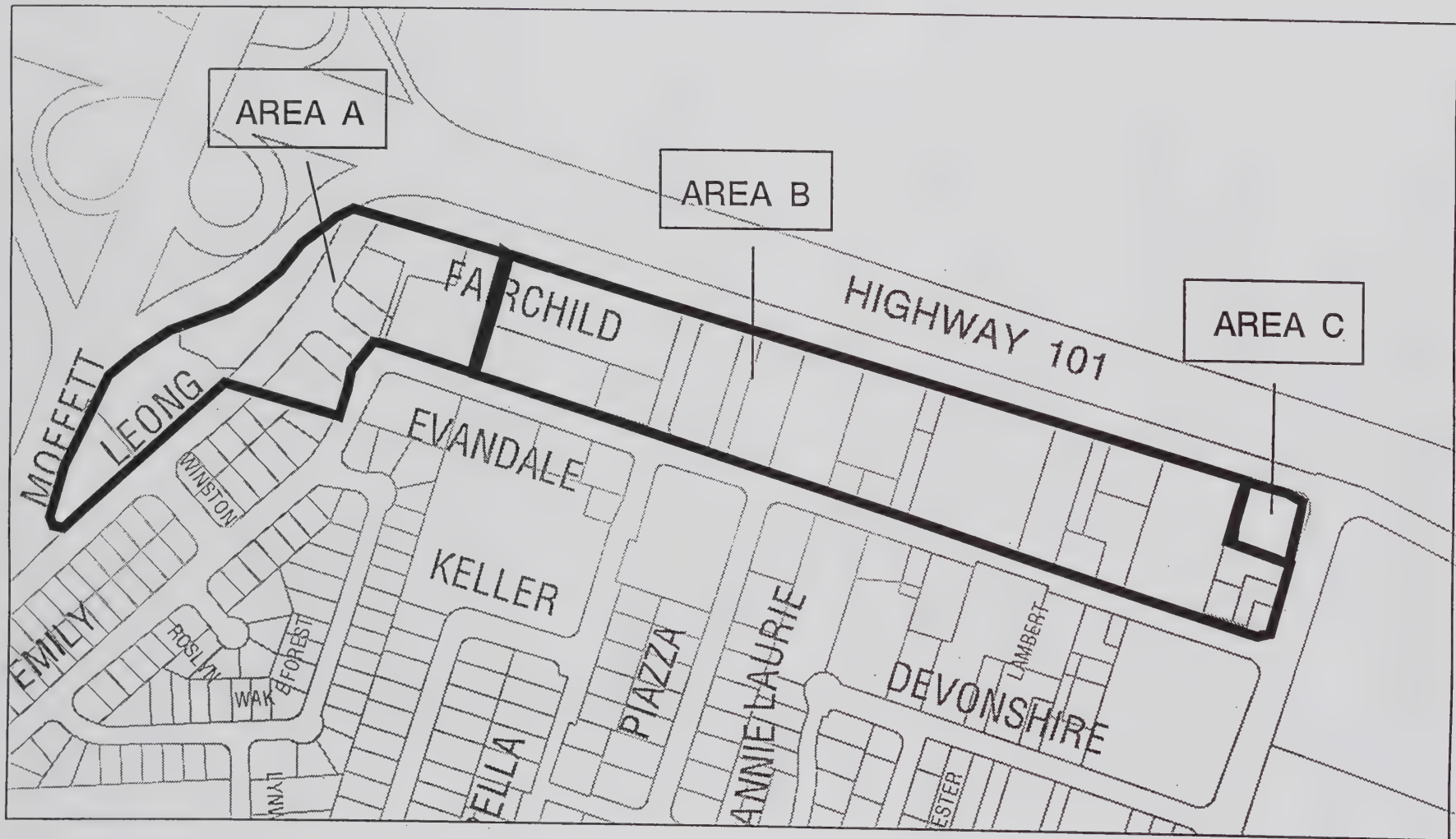
A. Location

The Evandale Area Precise Plan is a narrow strip of land adjacent to U.S. 101 between Moffett Boulevard and North Whisman Road. It is divided into three areas based primarily on former zoning.

B. Three Areas

- 1. *Area A* includes the former Leong-Fairchild Area Precise Plan area and the “island” between Leong Drive and Moffett Boulevard. The former Leong-Fairchild Area includes seven parcels (about 2.5 acres) that wrap around the corner of Fairchild and Leong Drives. In 1997, four older commercial buildings front on Leong Drive and a newer motel fronts on Fairchild Drive. The “island” includes three businesses (a motel, restaurant and gas station on a total of 3 acres) built in the early 1970s. Some of the commercial buildings in Area A provide neighborhood commercial services, but others serve the larger community.

EVANDALE PRECISE PLAN



2. *Area B* (formerly the Fairchild-Evendale Precise Plan area) covers lands bounded by Fairchild Drive on the north, North Whisman Road on the east, Evendale Avenue on the south, and Area A (formerly the Leong-Fairchild Precise Plan area) on the west. Area B covers ±15.2 acres of land. Older structures in varying condition dominate this area. About two-thirds of the land is in residential use and the remainder is commercial. Development facing Fairchild Drive in 1997 consists of motels, a landscape contractor, a small grocery, and various types of residential uses ranging from a mobile home/travel trailer park to apartment buildings. The remaining development in the block faces Evendale Avenue and is predominantly residential, including single-family, duplex and apartment units. These older rental units are a source of low and moderate income housing in Mountain View. When Area B was annexed to the City in 1954, the commercial uses became nonconforming. They later became legal, conforming uses under the Fairchild-Evendale Precise Plan (adopted in 1988).
3. *Area C* is a half-acre parcel at the corner of Whisman and Fairchild (299 Fairchild Drive) which, in 1997, was being developed with a new office building. This property was also in the Fairchild-Evendale Precise Plan, but it is the only parcel in that Precise Plan that has always been zoned for commercial use.

III. AREA A

A. Objective

1. Strengthen and unify this area as a neighborhood commercial center that serves the larger Whisman Area.

B. Allowed Uses

1. Neighborhood commercial uses permitted as principal, accessory and conditional uses in the Neighborhood Commercial (CN) zone district.
2. Motels and hotels.

C. Development Standards

1. The development standards for the Neighborhood Commercial (CN) zone shall apply with the following additional requirements.

2. Pedestrian Entrances—New buildings shall have at least one entrance facing the residential area, or the site design shall incorporate well-lighted, attractive pedestrian pathways from the residential area to other entrances. Existing buildings are strongly encouraged to develop similar entrances in order to foster the orientation of this center towards the residential areas.
3. Parking—Parking shall conform to Section 36.37 of the Zoning Ordinance including the following provisions regarding reductions in off-street parking:
 - a) The Zoning Administrator may grant a Conditional Use Permit for a reduction in off-street parking requirements if multiple uses or properties cooperatively establish and operate the facilities and if the uses generate their primary parking demands during different hours of the day. Approval shall be pursuant to Section 36.37.050.
 - b) Parking areas for any given lot or building shall be designed to encourage mutual access for both vehicles and pedestrians on adjacent lots or buildings.
4. Landscaping—A minimum of 15 percent of each parcel shall be landscaped and this shall include an 8-foot wide landscape strip behind the front property line. Mutual development of landscape areas is encouraged.
5. Signs—Signs are permitted in accordance with the CN zone with the following special provisions:
 - a) New signs shall comply with the criteria in Exhibit A-1, Sign Program for Area A, unless a majority of the property owners in Area A agree to an alternate sign program addressing the same design elements (materials, colors, use of graphics, etc.). Any such alternate sign program proposed by a majority of property owners shall become effective upon approval by the City pursuant to Section III.A, and, upon approval, shall replace the then existing sign program.
 - b) One freestanding center identification sign is permitted in accordance with size provisions of Section 36.14.7(b)(2) of the Zoning Ordinance to serve this entire commercial area provided an agreement is signed by a majority of property

owners in the center prior to approval of said sign, and the existing sign at 870 Leong Drive is removed. Such statement shall also contain provisions noting that this sign is the only freestanding sign allowed within the area.

IV. AREA B

A. Principles and Objectives

1. Strengthen the sense of neighborhood within the area and integrate it into the larger Whisman residential community.
2. Encourage residential redevelopment of the nonresidential and lower-density residential sites existing in 1997.
3. Provide incentives for the preservation and construction of low and moderate income housing.
4. Ensure that new residential development is protected from free-way noise.

B. Allowed Uses

1. Permitted Uses—Residential uses at the following densities:
 - a) For sites less than 2.5 acres—The maximum density shall be 20 to 25 units per acre. The maximum density shall be increased to 26 to 30 units per acre if at least 20 percent of the units are set aside as housing for lower-income households, or 10 percent for very-low income households, or 50 percent elderly.
 - b) For sites greater than 2.5 acres—The maximum density shall be 26 to 30 units per acre. The maximum density shall be increased to 31 to 38 units per acre if at least 20 percent of the units are set aside as housing for lower-income households, or 10 percent for very-low income households, or 50 percent elderly.

- c) The number of units allowed on a site is determined by the minimum lot area required for each dwelling as presented in the following tables:

20 to 25 Units per Acre

<u>One Unit</u>	<u>Two Units</u>	<u>Three Units</u>	<u>Four Units</u>	<u>Five Units</u>	<u>Additional Units</u>
5,000 SF	9,000 SF	12,000 SF	14,000 SF	15,500 SF	1,500 SF per Unit

26 to 30 Units per Acre

<u>One Unit</u>	<u>Two Units</u>	<u>Three Units</u>	<u>Four Units</u>	<u>Five Units</u>	<u>Additional Units</u>
5,000 SF	9,000 SF	12,000 SF	14,000 SF	15,250 SF	1,250 SF per Unit

31 to 38 Units per Acre

<u>One Unit</u>	<u>Two Units</u>	<u>Three Units</u>	<u>Four Units</u>	<u>Five Units</u>	<u>Additional Units</u>
5,000 SF	9,000 SF	12,000 SF	14,000 SF	15,000 SF	1,000 SF per Unit

2. Accessory Uses

- a) Accessory uses in buildings normally incidental to multiple-family uses. This shall not be construed as permitting any commercial use or occupation other than those specifically listed.
- b) Customary incidental home occupations subject to the provisions of Section 36.39.3 of the Zoning Ordinance.

3. Conditional Uses

- a) Boarding or lodging houses, nursing homes, convalescent hospitals and similar residential uses not specifically listed, which normally have a level of activity or population density that is no greater than the allowed uses.

- b) Child care centers.
- c) Motels and hotels on parcels that are adjacent to Area C.

C. Development Standards

1. The site development standards of the R3* District (Sections 36.11.4 through Section 36.11.13) shall apply to all principally permitted or conditionally permitted uses. (See Exhibit A-2.)
2. Detached units (single-family houses) are allowed only on the half-block facing Evandale Avenue.
3. In addition to the open space requirements in the R3 District, developments with 12 or more units shall provide a children's play apparatus area. Additional recreation spaces and/or facilities (as defined below) shall be provided for larger developments up to a maximum of four recreation spaces and facilities (including the play apparatus) for developments with 80 or more units. The types of recreation spaces and facilities considered to be usable recreation space are commonly shared children's play apparatus, turfed and generally level play fields, landscaped family picnic areas, game court areas, swimming pools, and recreation center building and grounds. Developments with private yards (with minimum dimensions of not less than 15 feet) may substitute the private yards for one of the required recreation spaces or facilities, except that the private yards cannot substitute for the children's play apparatus. All usable recreation space shall be buffered from freeway noise to the extent it is possible.
4. Residential units which have been provided as lower and very low income units pursuant to Paragraph B.1.a) and b). above shall be retained for a minimum of 30 years. Procedures for assuring their continued availability shall be approved by the City.
5. Noise attenuation shall be an important consideration in the site and architectural design of all projects. The following guidelines and standards address noise attenuation:
 - a) Noise levels of no greater than 45db(A)Ldn interior and 55db(A)Ldn exterior are encouraged to the maximum extent possible, although it is recognized that the exterior noise standards probably cannot be met throughout the site. All

developers must submit a report prepared by a qualified noise analyst projecting the noise levels to be present upon completion of any proposed development.

- b) Buildings, open space, parking, and landscaping shall be arranged in such a way as to maximize noise attenuation and to obtain relatively quiet outdoor usable recreation areas associated with as many units as possible. Placement of garage or carport facilities on the north (freeway) side is encouraged as an effective method for noise attenuation at the ground level. Other creative methods are encouraged.
 - c) Noise buffers shall be designed to combine with adjacent buffers to provide maximum noise attenuation and reduce echo effects.
 - d) Noise attenuation shall be a factor in determining acceptable architectural design and materials (e.g., stone, brick, etc.). However, aesthetic concerns should not be compromised (e.g., overuse of concrete block) in seeking to achieve this objective. Projects will be required to retain a residential appearance and character.
 - e) Interior structure layouts are encouraged to place active areas (kitchen, dining room) on the north sides of the structure, and passive (bedroom, living room) on the south, with proper consideration to be given to solar access to appropriate rooms.
- 6. North property lines of all parcels shall be heavily landscaped with coniferous and evergreen plant materials to provide a visual and dust buffer to the Bayshore Freeway.
 - 7. Unit design shall emphasize a positive and open relationship to the broader Whisman neighborhood.

D. Additional Design Standards for Hotels and Motels

- 1. Hotels and motels shall be residential in appearance. Dominant exterior materials shall be limited to wood, brick, stone, stucco, or like materials as approved by the Zoning Administrator. Architectural styling shall include but not be limited to pitched

roof lines, eaves, small-scale facade design elements, facade breaks, residential window and door placements, framing and inset, and ornamental detailing.

2. Site layout shall be compatible with residential uses on all sides. If the development has frontage on Evandale Avenue, it must have a positive and open relationship to Evandale Avenue.
3. There shall be no vehicular access from Evandale Avenue.
4. Landscape and open space
 - a) Minimum open green area: 25 percent of the net site area
 - b) Minimum landscape setbacks
 - (1) Front and Rear: 15' minimum
 - (2) Side: 5' minimum width planted with a continuous band of evergreen trees with expected height at least equal to the proposed building height, or 15' minimum with continuous landscaping.
5. Buffer walls: A minimum 7' acoustically designed and constructed noise wall shall be provided on any property line adjacent to a residentially developed or zoned property.
6. Special attention shall be given to the design and placement of parking areas, lighting, trash collection facilities, and delivery/maintenance areas to reduce impact on surrounding residential properties.
7. Signs:
 - a) Signs are permitted in accordance with the ML zone with the following special provision:
 - (1) No signs shall be permitted that are specifically oriented toward the Bayshore Freeway for the purposes of attracting business from the freeway.

V. AREA C

A. Principles and Objectives

1. Provide for a transition between the industrial area east of Whisman Road and the residential uses in Area B.

B. Allowed Uses

1. Office, research and development and warehouse uses except those which use the types and quantities of extremely hazardous materials defined in the Zoning Ordinance (Section 36.3, Definitions).

C. Development Standards

1. The design shall be residential in appearance and site layout shall be compatible with adjacent residential uses.
2. Floor Area Ratio shall not exceed 0.30.
3. Height shall not exceed two stories or 35', whichever is less.
4. Setbacks: 15' minimum front, side and rear
5. Landscape and open space
 - a) Minimum open green area: 25 percent of the net site area.
 - b) Minimum landscape setbacks
 - (1) Front and Rear: 15' minimum
 - (2) Side adjacent to residential: 5' minimum width, planted with tall-growing evergreen trees with expected height at least equal to proposed building height or 15' minimum width with continuous landscaping.
 - c) North property lines shall be heavily landscaped with tall-growing evergreen trees and mounded landscape areas to provide a visual and dust buffer to the Bayshore Freeway.
6. Parking shall comply with Section 36.37 of the Zoning Ordinance.

7. Maximum noise levels of 55 db(A)Ldn interior, 65 dB(A)Ldn exterior daytime and nighttime are encouraged to the maximum extent possible as determined by the Zoning Administrator.
8. A minimum 7' acoustically designed and constructed noise wall shall be provided on any rear or side property line adjacent to a residentially developed or zoned property.
9. Special attention shall be given to the design and placement of parking areas, lighting, trash collection facilities, and delivery/maintenance areas to reduce impacts on surrounding residential properties.
10. No exterior equipment, vents, or fans shall be permitted except for normal building heating and air conditioning. Any permitted equipment, vents or fans shall be located and oriented away from adjacent residentially zoned properties.
11. Signs are permitted in accordance with the ML zone with the following special provision:
 - a) No sign shall be permitted that is specifically oriented toward the Bayshore Freeway or for the purposes of attracting business from the freeway.

VI. ADMINISTRATION

- A. **Approvals**—All development within the Precise Plan Area shall be subject to approval by the Zoning Administrator pursuant to Sections 36.22.6 through 36.22.8 of the Zoning Ordinance. Minor site changes and building alterations, including material changes and changes in use which are in conformity with the Precise Plan may be authorized through the Site Plan and Architectural (SPAR) process.
- B. **Nonconforming Uses and Buildings**— Buildings and uses that become nonconforming by virtue of adoption of this Precise Plan shall be subject to Section 36.29 of the Zoning Ordinance (Nonconforming Uses and Nonconforming Structures) except that nonconforming uses and nonconforming structures shall be terminated within twenty (20) years of the date of adoption of this Precise Plan.
- C. **California Environmental Quality Act**—All proposals for development shall be subject to the mitigation measures specified in the Initial Study and Negative Declaration adopted by the City Council on December 9, 1997. A summary of the mitigation measures is attached.

SIGN PROGRAM FOR AREA A

The purpose of the sign program is to provide for a coordinated and visually attractive sign program that will enhance the neighborhood shopping area and adequately identify and define the individuality of the different tenants while providing an overall cohesive appearance for the neighborhood.

SIGN SPECIFICATIONS

Freestanding Center Identification Sign

1. One freestanding neighborhood center identification sign is permitted to serve the entire commercial area, at a location to be determined. The freestanding sign should either be housed in a frame or set on a base, creating a solid and attractive appearance.
2. The maximum height of the freestanding sign shall be 20'.
3. The freestanding sign should be located in a landscaped area that is equal to, or larger than, the total sign area.
4. Only one freestanding sign is permitted in this commercial area.

Building-Mounted Signs

1. Each tenant shall be permitted one building-mounted sign per occupancy frontage.
2. A tenant identification sign shall consist of individually mounted letters, no greater than 24" in height. The letters may be nonilluminated, interior illuminated, or backlit. Letters shall be mounted directly on the building or on a panel painted to match the building. The colors of the sign face shall be at the discretion of the tenant but should generally be limited to two colors.
3. Decorative graphics, including, but not limited to, company logos, are allowed in addition to the permitted building-mounted sign, subject to Site Plan and Architectural Review. The graphics must be clearly secondary to the main sign in terms of their size and the portion of the area covered and must complement the building and main sign in color and style. The area of the graphics is counted as part of the total sign area allowed on the building.

4. The aggregate sign area shall not exceed one-half (0.5) square foot per foot of occupancy frontage; provided, however, that any such sign may have an area of 15 square feet, and no sign shall exceed an area of 100 square feet.
5. All tenant identification signs shall be visually centered relative to the tenant space, face of the building or front door.
6. All returns shall be anodized or spray-painted aluminum with a matching trim cap detail. All conduit, light sources, conductors, transformers and other equipment shall be concealed from view. Exposed or visible neon tubing is not permitted. All bolts, fasteners, clips, etc. shall be painted out to match adjacent finish or match adjacent material and finish.
7. All electrical signs shall bear the UL label, and their installation must comply with all local building and electrical codes.
8. All signs shall be attached directly to the building. No exposed conduit, tubing or raceways shall be permitted.
9. All penetrations of the building structure required for sign installation shall be neatly sealed in a watertight condition.

Pedestrian Signs

1. Each tenant is allowed one pedestrian-oriented sign, not to exceed an area of 6 square feet for each face.
2. The pedestrian sign may be attached directly to the building or may be suspended from the canopy, eave or other overhanging architectural feature at a minimum height of not less than the minimum required door height above the surface of the sidewalk or pedestrian way.
3. The pedestrian sign may be provided with indirect or interior illumination.

PROHIBITED SIGNS

Signs with flashing or moving lights, animated, audible or moving signs, rooftop signs and cabinet signs are prohibited.

No signage shall be permitted that is specifically oriented toward the Bayshore Freeway or for the purposes of attracting business from the freeway.

NONCONFORMING SIGNS

The existing freestanding sign at 830 Leong Drive may remain as an approved part of the service station use regardless of other freestanding signs in this area.

The freestanding sign existing at 870 Leong Drive may remain unless it is replaced by a freestanding center identification sign.

EXCERPT—ZONING ORDINANCE— R3* DISTRICT REGULATIONS

SEC. 36.11. Multiple-family residential district or R3* district.

Purpose. This district is designed to provide for a range of multiple-family residential densities offering a wide choice of living accommodations for families of diverse composition and lifestyle. It is intended to afford generous open spaces in close proximity to each dwelling unit as a means of enhancing the quality of life. The district is also intended in special cases to allow appropriate community services. (Ord. No. 29.66, 12/12/66; Ord. No. 34.73, 10/29/73; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.1. Principal permitted uses.

- (a) Single-family dwellings and two-family dwellings or duplexes.
- (b) Multiple-family dwellings and apartment houses.
- (c) Townhouse developments, when found to be in substantial compliance with Design Guidelines for Townhouse Development.
- (d) Crop and tree farming. (Ord. No. 10.89, 5/9/89.)

SEC. 36.11.2. Accessory uses permitted.

- (a) Accessory uses and buildings normally incidental to any of the above. This shall not be construed as permitting any commercial use or occupation other than those specifically listed.
- (b) Off-street parking lots when appurtenant to any permitted use, subject to the provisions of Sec. 36.37 through 36.37.2.
- (c) Customary incidental home occupations subject to the provisions of Sec. 36.39.3.
- (d) Offices incidental and necessary to the conduct of a permitted use.

(e) Child-care centers as an accessory use in a church. (Ord. No. 27.66, 11/14/66; Ord. No. 34.73, 10/29/73; Ord. No. 10.89, 5/9/89; Ord. No. 12.95, 5/30/95.)

SEC. 36.11.3. Conditional uses.

(a) Boarding or lodging houses, nursing homes, convalescent hospitals and similar residential uses which are no more intense in character than the intensity of use set forth under Sec. 36.11.7, Density. (Ord. No. 14.71, 4/26/71; Ord. No. 34.73, 10/29/73; Ord. No. 10.89, 5/9/89.)

(b) Public halls, lodges and clubs.

(c) Public and quasi-public buildings and uses of a recreational, educational, religious, cultural or public service type, including public utility buildings, structures and uses; but not including corporation, storage or repair yards, warehouses and similar uses.

(d) Public or private parking lots for automobiles when contiguous to any C, M, O, or P district, subject to the provisions of Sec. 36.37 through 36.37.2.

(e) Repealed by Ord. No. 39.74, 11/25/74.

(f) Child-care centers. (Ord. No. 10.89, 5/9/89; Ord. No. 12.95, 5/30/95.)

SEC. 36.11.4. Height regulations.

(a) **Height regulations for single-family dwellings.** For a lot occupied or to be occupied by one single-family dwelling, the height shall be in accordance with the provisions of Sec. 36.9.4.

(b) **Height regulations for a duplex or two detached dwellings.** For a lot occupied or to be occupied by a duplex or two detached dwellings, the height shall be in accordance with the provisions of Sec. 36.10.4.

(c) **Height regulations for multiple-family dwellings and townhouse developments.** Except as provided in Sec. 36.40.1, no portion of any multiple-family buildings shall exceed thirty-five (35) feet (10.67 m.) in height. (Ord. No. 34.73, 10/29/73; Ord. No. 10.83, 4/26/83; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.5. Area, lot width and yard requirements.

(a) **Lot area.** All lots shall contain a minimum of eight thousand (8,000) square feet (736 sq. m.).

(b) **Lot width.** All lots shall have minimum width of eighty (80) feet (24.38 m.) or a width equal to one-third of lot depth, whichever is greater. However, no lot need be in excess of two hundred (200) feet in width.

(c) An existing lot of record having a lot width of less than eighty (80) feet (24.38 m.) shall not be developed with more than two (2) dwelling units.

(d) **Front yard.** There shall be a front yard of at least fifteen (15) feet (4.57 m.) which yard shall be increased so that the area of that portion of the front yard lying in front of the building shall be not less than the area of the front wall or walls of the building. The front wall or walls shall be deemed to be any wall located within thirty-five (35) feet (10.76 m.) of the front property line.

(e) **Side yards.**

(1) **Side yards for single-family dwellings.** For a lot occupied or to be occupied by one single-family dwelling, each side yard shall be in accordance with the provisions of Sec. 36.9.5(d).

(2) **Side yards for a duplex or two detached dwellings.** For a lot occupied or to be occupied by one duplex or two detached dwellings, the side yards shall be in accordance with the provisions of Sec. 36.10.5(d).

(3) **Side yards for multiple-family dwellings or dwelling groups and townhouse developments.** There shall be a side yard of at least fifteen (15) feet (4.57 m.) which yard shall be increased so that the portion of the side yard at that point shall be not less than the height of wall of any building at that point.

(f) **Rear yards.**

(1) **Rear yard for a single-family dwelling.** For a lot occupied or to be occupied by one single-family dwelling, the rear yard shall be in accordance with the provisions of Sec. 36.9.5(e).

(2) **Rear yard for a duplex or two detached dwellings.** For a lot occupied or to be occupied by one duplex or two detached dwellings, the rear yard shall be in accordance with the provisions of Sec. 36.10.5(e).

(3) **Rear yard for multiple-family dwelling or dwelling groups and townhouse developments.** There shall be a rear yard of at least fifteen (15) feet (4.57 m.) which yard shall be increased so that the portion of the rear yard at that point shall be not less than the height of wall of any building at that point. (Ord. No. 34.73, 10/29/73; Ord. No. 10.83, 4/26/83; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.6. Special yards and distances between buildings on the same lot.

(a) The distance between a principal building and any accessory building shall be at least equivalent to the height of wall of the accessory building measured at the closest wall facing the principal building; however, in no case shall the distance be less than ten (10) feet (3.04 m.).

(b) The distance between principal buildings shall be at least one-half the sum of the heights of the nearest opposing walls; provided, however, that in no case shall the distance be less than twelve (12) feet. This requirement shall also apply to portions of the same building separated from each other by a court or other open space. (Ord. No. 34.73, 10/29/73; Ord. No. 10.83, 4/26/83; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.7. Density.

(a) The minimum lot area required to construct multi-family dwellings and apartment houses shall be in accord with one of the following zone designations:

Required Square Footage for Each Dwelling Unit

(1,000's of Square Feet)

Zone	1st Unit	2nd	3rd	4th	5th	Each Additional Unit
R3-1	5	4	3	2	1	1
R3-1.25	5	4	3	2	1.25	1.25
R3-1.5	5	4	3	2	1.5	1.5
R3-2	5	4	3	2	2	2
R3-2.2	5	4	3	2.2	2.2	2.2
R3-2.5	5	4	3	2.5	2.5	2.5
R3-3	5	4	3	3	3	3

However, existing lots of record having an area of seven thousand (7,000) square feet or more shall have the right to two (2) dwelling units and any legal lot in this zone is entitled to one (1) dwelling unit subject to all other provisions of this section.

(b) The standard density for townhouse developments in any multi-family district is a maximum of 12 units per acre, unless the underlying density is more restrictive (i.e., fewer units).

(c) A density not to exceed fourteen (14) units per acre may be granted by the zoning administrator for townhouse developments if findings can be made that the project complies with criteria for bonus density, as set forth in the design guidelines for townhouse development. (Ord. No. 39.63, 3/11/63; Ord. No. 29.66, 12/12/66; Ord. No. 34.73, 10/29/73; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.8. Lot coverage.

Buildings or structures shall not cover more than thirty-five (35) percent of the total lot. (Ord. No. 10.89, 5/9/89.)

SEC. 36.11.9. Floor area ratio (FAR).

(a) The sum of the gross floor areas of all buildings in multi-family and apartment houses shall not exceed one hundred five (105) percent of the total lot area for a FAR of 1.05.

(b) The sum of the gross floor areas of all buildings in townhouse development shall not exceed fifty-five (55) percent of the total lot area for a FAR of 0.55. (Ord. No. 10.89, 5/9/89.)

SEC. 36.11.10. Parking coverage.

(a) Open automobile-dedicated area shall not exceed twenty (20) percent of the total lot area for multi-family dwellings and apartment houses.

(b) Open automobile-dedicated area shall not exceed twenty-five (25) percent of the total lot area for townhouse developments. (Ord. No. 10.89, 5/9/89.)

SEC. 36.11.11. Open green area.

(a) Open green area for multi-family and apartments shall occupy no less than fifty-five (55) percent of the area of the lot.

(b) Open green area at grade level for townhouse developments shall occupy no less than forty-five (45) percent of the area of the lot. (Ord. No. 10.89, 5/9/89.)

SEC. 36.11.12. Special regulations.

- (a) Site plan and architectural approval as provided in Sec. 36.50 through 36.50.8.
- (b) Special attention shall be given to the inclusion and design of usable recreation space in projects designed for or capable of accommodating families with children.
- (c) Townhouse developments shall be evaluated according to the provisions of the Design Guidelines for Townhouse Development. (Ord. No. 34.73, 10/29/73; Ord. No. 10.89, 5/9/89.)

SEC. 36.11.13. Signs.

- (a) The provisions of Sec. 36.41 through 36.41.11, Signs, shall apply.
- (b) **Permitted signs.**
 - (1) **Identification signs. Single-family dwellings and duplexes.**

One (1) identification sign shall be permitted for each occupancy. The said sign shall not exceed an area of one (1) square foot, shall not exceed a height of nine (9) feet above the surface of the street, shall be attached directly to a building, fence, standard, or mail box, and shall be unlighted, or provided with indirect or interior illumination.
 - (2) **Identification signs. Multiple-family dwellings, apartment houses and dwelling groups.**
 - (i) **Aggregate sign area.** The aggregate sign area for any lot shall not exceed five (5) square feet for each acre or portion thereof within the lot; provided, however, that any lot may have an area of twenty-five (25) square feet and no one sign shall exceed an area of fifty (50) square feet.
 - (ii) **Permitted signs.** One (1) identification sign shall be permitted for each lot. The said sign may be attached flat against a principal building, in which event it shall not project above the eave of the roof or the top of the parapet, or may be free-standing, in which event it shall not be located within ten (10) feet of any public right-of-way and shall not exceed a height of ten (10) feet. Dwelling groups shall be permitted one (1) additional sign for each building. Said additional sign shall be attached flat against the said building and shall not project above the eave of the roof or the top of the parapet. Any such signs shall be unlighted or provided with indirect or interior illumination and shall be subject to the provisions of subparagraph (i) of this section.

(3) **Identification signs appurtenant to conditional uses.**

Contemporaneous with or subsequent to the granting of a conditional use permit pursuant to Sec. 36.43 through 36.43.6 of this chapter, the zoning administrator may authorize the erection of identification signs appurtenant to such conditional use and may impose restrictions on their size, height, lighting, shape, color and location. Signs for townhouse developments shall generally follow the sign provisions for multi-family developments. The obtaining of the said authorization and the erection of the said signs in conformity with the said restrictions shall constitute conditions to the exercise of the said conditional use permit, and any request for the said authorization made subsequent to the granting of the said conditional use permit shall be deemed a further hearing on the said permit and shall be subject to the same procedural requirements as the initial hearing.

(4) **Real estate signs. Single-family dwellings and duplexes.**

Two (2) real estate signs shall be permitted for each lot, of which one (1) sign shall be for sale purposes and one (1) sign for lease purposes. Each sign may have two (2) faces, shall not exceed an area of four (4) square feet per face, shall not exceed a height of five (5) feet above the surface of the street, unless placed in a window, and shall be unlighted.

(5) **Real estate signs. Multiple-family dwellings, apartment houses, dwelling groups and townhouse developments.**

One (1) real estate sign shall be permitted for each lot. The said sign shall not exceed an area of twelve (12) square feet, shall be attached flat against a principal building, shall not project above the eave of the roof or the top of the parapet of the said building, and shall be unlighted.

WHISMAN AREA PLAN PARKS AND OPEN SPACE ELEMENT

OBJECTIVES

- A. Increase the amount of parkland serving neighborhood residents.
 - Find a site for a mini-park in the **Tyrella/Evandale** neighborhood.
 - Consider other possible open space opportunities such as the *orchard property* next to the City's Municipal Operations Center on Whisman Road.
 - B. Develop a trail, and possibly a park with community gardens, on the Hetch-Hetchy right-of-way where it passes through the Whisman neighborhood.
 - C. Create a comprehensive system of bicycle and pedestrian paths connecting the industrial and residential areas with one another and with other parts of the City.
-

BACKGROUND

Neighborhood Workshop

Improving parks in the Whisman Area was a key concern of the June 1996 neighborhood workshop. Two specific recommendations were made:

- Make use of the Hetch-Hetchy right-of-way for a trail, or perhaps a park and community gardens.
- Develop a mini-park in the Evandale Avenue area.

Relationship to City-Wide Park Planning

Both of the park suggestions are supported by the City's Parks and Open Space Plan. This Plan is the guide to park needs and park planning in Mountain View. It is the link between the General Plan's open space goals and policies and the City's five-year Capital Improvement Program (CIP) which provides the funding for specific park projects. The Parks and Open Space Plan establishes priorities for park acquisition and

development and is updated every two years to reflect changing community needs and new opportunities.

The Plan was updated in 1995 and was undergoing another scheduled review in 1997. The 1997 review, conducted by the Parks and Recreation Commission (PRC), was coordinated with the Whisman Area Plan. The PRC's 1997-98 Parks and Open Space Plan Review was approved by the City Council in early 1998. The Parks and Open Space Plan establishes priorities for park acquisition and development based on the PRC's assessment of the relative needs of all of the neighborhoods in the City. Based on housing densities and lack of public open space, the "Tyrella/Evandale" neighborhood, within the larger Whisman area, was ranked as one of three neighborhoods in the City with the greatest need for a park in 1995. This ranking supported the recommendation of the neighborhood workshop. In early 1998, the City purchased a mini-park site on Devonshire which is in the target area. (The Tyrella-Evandale area is not specifically defined other than by the street names, but it is in the northerly section of the Whisman residential area.)

The 1997-98 Parks and Open Space Plan gives high priority to developing the Hetch-Hetchy right-of-way for biking and other recreational opportunities. The Hetch-Hetchy right-of-way is an 80-foot wide strip of land owned by the City of San Francisco that crosses the City of Mountain View from east to west. Large pipes carrying water from the Hetch-Hetchy Reservoir in the Sierra Nevada are buried beneath its surface. Many segments are leased to adjacent property owners for parking lots and other uses. Permanent structures are not allowed. The Whisman Area offers one of the best opportunities in the City for trail use because the right-of-way is generally unobstructed by long-term leases. Between Whisman Road and Tyrella Avenue, the right-of-way widens to 250 feet, creating a large open area in the middle of the block. In 1997, this space is leased to a private nursery to grow plants in containers. The section between Tyrella Avenue and Easy Street is vacant. Between Easy Street and Stevens Creek, it is a part of Whisman Park.

The 1997-98 Parks and Open Space Plan also includes a comment that the orchard lands adjacent to the City's Municipal Operations Center on Whisman Road offer future potential for open space. A use that would serve the entire City is to acquire the site for an historic orchard or farm, preserving the trees and integrating related uses such as community gardens.

Existing and Planned Parks and Open Space in the Whisman Area

Schools. In the 1997-98 Parks and Open Space Plan, the only open space listed for the Whisman Area was at the two schools: Slater School south of Middlefield Road and Whisman School north of Middlefield. The open space at Whisman is actually a joint city/school park with the City owning 2.2 acres. The sites provide playfields for soccer, baseball and softball as well as play equipment. There are also four tennis courts and a public rest room at Whisman School/Park.

Stevens Creek Trail. The Stevens Creek Trail, a major new north-south bicycle and pedestrian corridor, parallels the western boundary of the Whisman area. When completed, the trail will extend from the North Bayshore industrial area to residential neighborhoods south of El Camino Real. In 1996, the trail had been completed from L'Avenida (with an undercrossing at U.S. 101) to Whisman School, providing Whisman Area residents with direct access to recreational opportunities at Shoreline Park in the North Bayshore Area. Construction of the next segment (Reach 3) began in 1997. This segment will give Whisman residents a bicycle/pedestrian connection to Downtown Mountain View via an undercrossing at Middlefield Road and a pedestrian/bicycle bridge that will span Central Expressway and the railroad tracks.

Easy Street. In May 1996, the City began the process of turning a vacant well site into a mini-park by holding a neighborhood design workshop. Based on the workshop ideas, a park designer drew up detailed plans. The park, on 0.5 acre, will include a small playground, picnic tables and lawn. A walkway will lead to a new bridge crossing Stevens Creek to the Stevens Creek trail. Construction is to begin in 1997.

Whisman Station. Approval of the Whisman Station housing development on the former GTE property created an unanticipated opportunity for additional parkland in 1996. To fulfill the City's park dedication ordinance, the development will provide two new parks. Each of them is about one acre and will be completed as the housing is constructed. One will be next to the Light Rail station and have benches, picnic tables and lawn. The other will be on the southeasterly side of the Whisman Station residential community and will have play equipment. Both parks are open to the general public and will be identified with signs on Whisman Road.

IMPLEMENTATION PLAN

Objective A. Increase the amount of parkland serving neighborhood residents.

- *Find a site for a mini-park in the Tyrella/Evandale neighborhood.*

The City purchased land on Devonshire for a mini-park in early 1998. The park site is in the Tyrella-Evandale neighborhood.

- *Consider other possible open space opportunities such as the orchard property next to the City's Municipal Operations Center on Whisman Road.*

This site may be appropriate for a community orchard and garden and other uses that commemorate the area's agricultural history.

Objective B. Develop a trail, and possibly a park, on the Hetch-Hetchy right-of-way where it passes through the Whisman neighborhood.

The Hetch-Hetchy right-of-way offers a unique opportunity for a shared bicycle/pedestrian pathway crossing the residential neighborhood from Whisman Road to the Stevens Creek Trail. It could also be the location of a future neighborhood park which would be the largest in the Whisman Area. The park could provide play structures, playfields, a community garden and other amenities depending on community support and desires.

Staff for the San Francisco Public Utilities Commission (PUC), which owns and manages the Hetch-Hetchy lands, has indicated receptivity to City of Mountain View use of the land for a trail shared with the nursery as shown on the following conceptual drawing. Use of the full site for a park is less certain, but remains a desirable objective for the remainder of the site. The conceptual drawing shows how a trail could be combined with park uses, if the San Francisco PUC is willing to make the land available at some point in time. Public review of this proposal is occurring with review of this plan and with the Parks and Open Space update process.

Objective C. Create a comprehensive system of bicycle and pedestrian paths connecting the industrial and residential areas with one another and with other parts of the City.

Development of the Hetch-Hetchy right-of-way into a trail and perhaps a park, combined with the pedestrian/bicycle circulation system in the industrial area, will ultimately create a comprehensive off-road circulation system for the entire Whisman Area. Medians proposed on Whisman and Middlefield Roads will contribute to the system by providing pedestrians who are trying to cross wide streets a "safe haven." From the Whisman neighborhood, it will be possible to walk and bicycle to the Light Rail Stations, jobs in the Ellis-Middlefield industrial area, recreational opportunities and jobs in the North Bayshore Area and Downtown Mountain View – almost entirely separate from auto and truck traffic. The complete system is shown on the following map.

SUMMARY OF OPEN SPACE RESOURCES

Mountain View aims for a park ratio of 3 acres per 1,000 people. In the Whisman Area, Slater School and Whisman School/Park provide about 1.7 acres per 1,000 people. With the addition of parks in the design or planning stage in the Whisman Area, and the increase in population from Whisman Station (the GTE site), the ratio would increase to 2.0 acres per 1,000 people (or 2.7 acres per 1,000 people if the Hetch-Hetchy right-of-way became a park).

Existing

Slater School	4.3 acres*
Whisman School and Park	8.5 acres*

In Design Stage

Easy Street Mini Park	0.5 acre
Whisman Station Parks	2.0 acres

Proposals

Hetch-Hetchy Right-of-Way	1.0 acre to 7.0 acres**
Mini-Park in Tyrella-Evandale Area	0.5 acre

Other

Stevens Creek Trail

Total	16.8 acres to 22.8 acres
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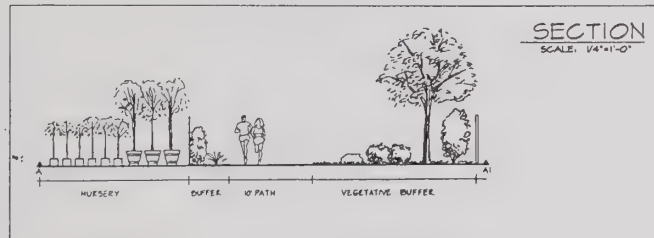
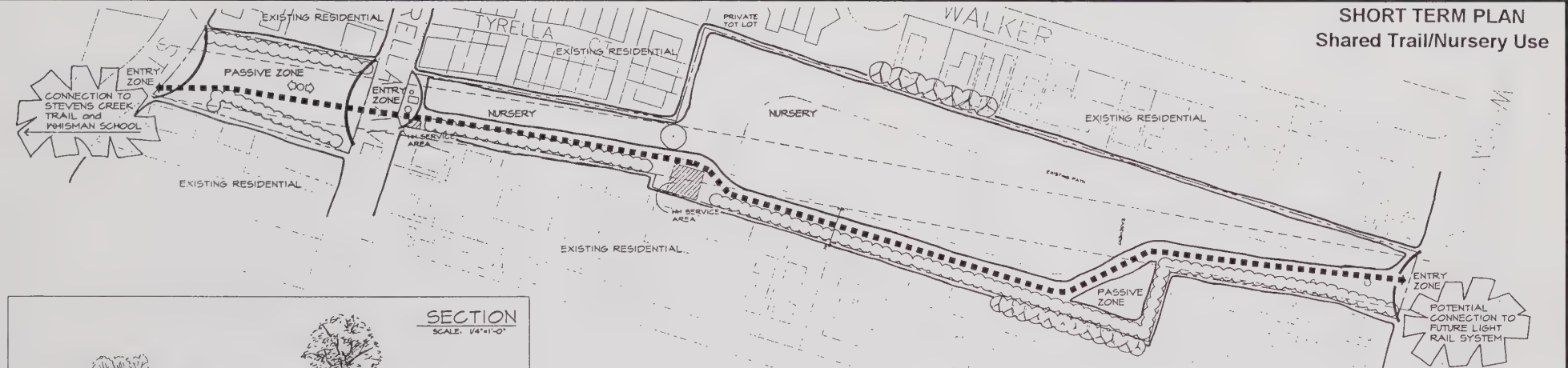
* Estimate. Open space area only

**About 1 acre if a trail only; 7 acres if trail and park



WHISMAN AREA Parks, Open Space and Trail Plan

SHORT TERM PLAN Shared Trail/Nursery Use



PASSIVE ZONE USES:

COMFORTY SHADING
OPEN SPACE
SMALL TOT LOT
BENCHES
PICNIC TABLE
PARKING

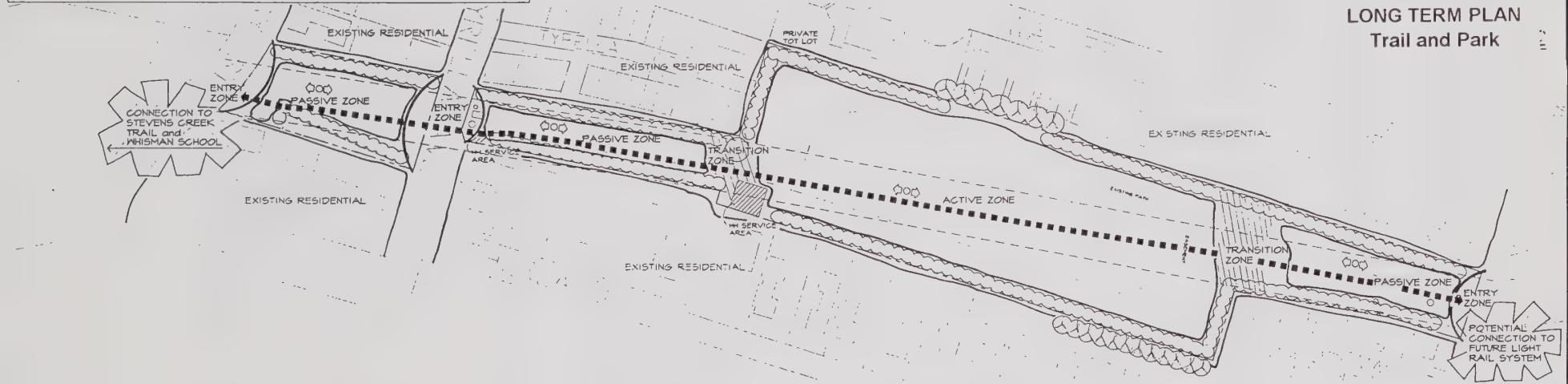
ACTIVE ZONE USES:

BALLFIELDS
PLAYGROUND
LARGE PICNIC AREAS

KEY

- SIGNIFICANT TREE
- VEGETATIVE BUFFER
- PEDESTRIAN-WALKWAY PATH AND SAFETY ACCESS
- EXISTING EVERGREEN SCREEN TREE
- VISUAL CORRIDOR

LONG TERM PLAN Trail and Park



April 8, 1997

HETCH HETCHY EASEMENT CONCEPTUAL PLAN
CITY OF MOUNTAIN VIEW, CALIFORNIA



LAUDERBAUGHILL
ARCHITECTS
Landscape Architecture/Planning
100 Fremont Street, Suite 300
San Francisco, California 94105-5070
(415) 774-7200

IMPROVING COMMUNITY APPEARANCE IN THE WHISMAN AREA

OBJECTIVES

- To encourage interest and involvement of area residents in their neighborhood.
 - To increase the visibility and effectiveness of City code enforcement programs in this area.
 - To target major blighting influences in the Whisman area for concentrated City code enforcement efforts.
-

BACKGROUND

In response to growing neighborhood concerns and problems, the City has initiated a new program called the Neighborhood Preservation Strategy (NPS). The overall goal of the NPS is to expand available community resources and to utilize the collective abilities of both the neighborhoods and the City to identify and implement solutions.

A basic premise of the NPS is that residents are the City's best resource in terms of understanding what is needed to preserve and improve the neighborhoods. The City's role as part of this process is to listen to neighborhood concerns and, where possible, to redirect resources to address these needs. The role of the neighborhood is to be involved in this process through neighborhood associations and/or to attend meetings sponsored by the City (i.e., Town Hall meetings or the neighborhood meetings sponsored by the City Council Neighborhoods Committee). A foundation of the NPS is that neighborhood revitalization is only effective when it first and foremost involves area residents. Successful neighborhood revitalization strategies emanate from a shared vision of both leaders in City Hall and in the neighborhood. The neighborhood and the City must work together. Neither party can, on its own, redress community blight, gangs and other social problems.

The City in recent years has undertaken a number of efforts to promote community involvement and participation in problem-solving on a City-wide basis as well as several efforts focused on the Whisman area. The special efforts in the Whisman area include:

- Four meetings of the Council Neighborhoods Committee over the past three years. These meetings have served to promote participation in the community and the neighborhood and to make area residents aware of City services, including code enforcement programs. There also have been meetings with area residents as part of the development of the Whisman Area Plan and special meetings with residents concerned about conditions in the Evandale/Fairchild area.
- The Police Department has carried out a number of Community-Oriented Policing actions in this area to deal with particular crime problems. Community-Oriented Policing is part of the City's Community Policing initiative, where Police staff takes a problem-solving approach to community concerns. Community-Oriented Policing has been particularly successful in dealing with drug problems near Highway 101 in this area. As a result of these efforts, the neighborhood has witnessed a substantial drop in drug complaints and violations.
- Special City code compliance efforts have been made in this area, principally involving the area in the vicinity of Fairchild Drive and Evandale Avenue. Staff have conducted comprehensive sweeps of this area and provided ongoing follow-up to deal with outstanding code compliance issues. City Housing Inspectors and the City Attorney's staff have worked to require owners of several apartment buildings to clean up their properties and to make needed health and safety corrections. A major effort was made to improve the appearances of the Lucky U and Moffett Motels (these efforts were successful in getting both buildings painted and a large amount of debris removed).
- The City conducted a special neighborhood cleanup in the Evandale/Fairchild area in 1996 which was well received and involved large numbers of area residents.
- Special efforts were made to organize residents in the Evandale/Fairchild area with several meetings with a core group of interested residents from this area.

The Whisman area is marked with a number of older buildings that display signs of neglect. Most of the neglect of properties in this area is not a code violation but is nevertheless of concern to many. There are also conflicts between the expectations of the many new residents of this quite diverse area and the older residents, some of whom

have different standards relating to acceptable neighborhood appearances. It is a challenge to work with this area, which to date has not developed a consensus on appropriate community appearance. An improved level of community participation coupled with active neighborhood associations would help resolve some of these conflicts.

IMPLEMENTATION PLAN

The No. 1 priority for this area is to stabilize and improve, where possible, areas showing the greatest levels of deterioration. This can best be accomplished with an active partnership of area residents and the City. The neighborhood needs more leadership that will provide a clear and stronger voice on neighborhood issues and represent the area's interest to the City.

Continued City involvement in the Whisman area will include:

- The City will continue to promote resident involvement in the area through neighborhood associations, Neighborhood Watch, Recycling Volunteers and other community involvement opportunities.
- The City will continue to enforce on a proactive basis the Rental Inspection Program (for all apartments and motels) with a focus on problem properties.
- The City code enforcement staff will carry out a comprehensive sweep of the Evandale/Fairchild area at least once a year for major blighting problems such as abandoned vehicles, weeds and debris problems.
- The Police Department will continue to carry out Community-Oriented Policing actions to deal with drug and other serious criminal manifestations.
- Staff will respond to any and all code complaints from area residents.
- The City, through the Neighborhood Preservation Strategy, will continue to meet with area residents to encourage their participation, to maintain an awareness of livability issues affecting this area and to assist them in forming active associations.
- The City will carry out special initiatives in this area such as neighborhood cleanups where there is a demonstrated local interest.

AMENDMENT TO ML DISTRICT (ADMINISTRATIVE, RESEARCH AND LIMITED INDUSTRIAL DISTRICT)

ADD THE FOLLOWING NEW LANGUAGE TO SECTION 36.19.1: PRINCIPAL PERMITTED USES.

- “(h) Restaurants, including incidental sale of beer and wine for consumption on the premises and retail and service uses to serve employees in the ML District that meet the following criteria:
1. The use is a minor, and clearly ancillary, part of a larger permitted or conditional use on a parcel.
 2. The total floor area of the commercial uses does not exceed 3,000 square feet.
 3. The use is not immediately adjacent to loading docks, outside storage and equipment areas or other activity areas with high noise levels, odors or other conditions undesirable for commercial uses.
 4. The use is not on a parcel, or adjacent to a parcel, where extremely hazardous materials, as defined in Section 36.3, Definitions, are used.
 5. Parking is provided at a ratio which is no less than the ratio required for the primary use on the site.
 6. Signs are permitted pursuant to Sec. 36.19.7. In addition, commercial uses permitted under this Section are allowed a freestanding sign with an area not to exceed ten (10) square feet.”

ADD THE UNDERLINED WORDS TO SECTION 36.19.3: CONDITIONAL USES.

- “(c) Appropriate retail commercial and service uses such as service stations necessary to serve the ML district other than provided in Section 36.19.1(h).”



City of Mountain View

Streetscape Guidelines for the Ellis-Middlefield District

July 16, 1997

City of Mountain View Streetscape Guidelines for the Ellis-Middlefield District

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City of Mountain View

Streetscape Guidelines for the Ellis-Middlefield District

Design Counts! Great streets do not just happen. Overwhelmingly, the best streets derive from a conscious act of conception and creation of the street as a whole. The hands of decision makers, sometimes of specific designers, are visible.

Allan B. Jacobs

INTRODUCTION

Streets and sidewalks are a public space accessible and visible to everyone. What we experience within this public place is an important part of the quality of life within a community. Streets and sidewalks perform the essential practical function of efficiently moving people from place to place. They also make an important social contribution by maintaining a common space that brings businesses together, creates the neighborhood fabric, and connects each part of the community.

Streetscape are those features that comprise the suburban experience in Mountain View: roads, curbs and gutters, medians, trees, sidewalks, lights, groundcover and shrubs, and the building walls and architectural features. Each street, district, and neighborhood has its own unique streetscape depending on the width of the street, type and amount of trees, and location and design of the buildings. In industrial districts, the streetscape is the front door for business, the place where clients get their first impression. It is also where employees come and go, exercise at lunch, walk to transit, and develop a sense of connection with the community.

The streetscape of the Ellis-Middlefield District is undergoing a transformation. In 1995, the Ellis-Middlefield industrial district began to reemerge as a premiere corporate location. Old industrial buildings have been replaced by almost 1 million square feet of new corporate office space. The Tasman Corridor Light Rail is accompanying this tremendous reinvestment and is scheduled to arrive in 2000, bringing with it new transportation options, increased pedestrian activity, and a new look to the area. The Streetscape Guidelines are intended to keep pace with the dynamic changes in this District by building upon the existing streetscape and seizing opportunities to improve.

Streetscape Guidelines for the Ellis-Middlefield District

GOALS AND OBJECTIVES

The Streetscape Guidelines contain standards for trees, landscaping, hardscapes, medians, pedestrian walkways, and private landscaping within the Ellis-Middlefield District. These standards are intended to implement the following goals and objectives.

- ◆ Build upon the best features of the existing streetscape to create a more attractive, cohesive identity for the district.
- ◆ Create more distinctive gateways by installing gateway features that distinguish the entries to the City and establish a strong identity for this industrial district.
- ◆ Implement features that will complement the new Tasman Corridor Light Rail and incorporate urban streetscape features around light rail stations, including plazas, tree wells, shady places to sit, and an integrated relationship with adjacent buildings. Consider a City of Mountain View sign where Light Rail enters the City.
- ◆ Provide new medians on major arterials to reduce broad expanses of pavement, create a more human scale, and provide opportunities to install distinctive gateway features.
- ◆ Complete sidewalks throughout the district and make the pedestrian experience comfortable, safe, and enjoyable by limiting driveways and adding trees, parkways, and places to sit.
- ◆ Provide new pedestrian walkways that create efficient, safe, easily identified connections to light rail stations and other major pedestrian and bicycle pathways within the City, such as the Stevens Creek Trail.
- ◆ Create an effective buffer and transition between the industrial district and adjacent residential neighborhoods that screens larger industrial buildings, maintains a human scale on the residential side, and helps protect neighborhoods from traffic, noise, and inappropriate lighting.
- ◆ Replace missing and unhealthy street trees to improve the pedestrian experience and screen industrial buildings and consider changes to the City's Master Street Tree List based on the goals of these Guidelines.
- ◆ Blend public and private landscaping within the streetscape to create a unified and harmonious appearance that enhances both public and private improvements.
- ◆ Ensure that private landscaping is well designed and includes sufficient variety and numbers of trees, shrubs and other plants to create a quality streetscape environment.

Streetscape Guidelines for the Ellis-Middlefield District

- ◆ Consider public art at gateways and major focal points to give importance to these locations and add interest and variety to the district.

IMPLEMENTATION

The Streetscape Guidelines shall be implemented on private property as those properties redevelop or apply for substantial modifications to the building exterior, parking areas, or landscaping. Improvements on public property shall be implemented as part of the City's annual Capital Improvements Program when funding becomes available and/or as part of private redevelopment or remodeling that may require improvement of public areas. Implementation of improvements for the Whisman-Middlefield focal point should be considered by the City as part of a Capital Improvements Program to ensure timely completion of all four corners.

EXISTING CONDITIONS IN THE DISTRICT

The Ellis-Middlefield District is a previously developed industrial district with mature landscaping, large blocks, and an established street system based on auto circulation rather than pedestrian movement. Most public improvements are complete. Figure A shows the boundaries of the District and some of the key features, including the future Tasman Corridor Light Rail line and pedestrian and bicycle pathway improvements. These existing conditions and future changes are the foundation for the improvements recommended in these Guidelines.

Surrounding Land Uses. The Ellis-Middlefield industrial district is bordered by two freeways that provide direct access to the district, Highway 101 and State Route 237. To the north, opposite Highway 101, is Moffett Field and the NASA/Ames facility. From Whisman Road and Ellis Street, views of the hangers at Moffett Field are dramatic due to their sheer size. To the west of this industrial district is a residential neighborhood that includes a mixture of single-family and multiple-family homes. Slater Elementary School is located along this western border on the residential side of Whisman Road. To the south, on the former GTE property, is the emerging residential neighborhood of 525 new single-family homes and townhomes.

A review of the existing residential neighborhood to the west found that the residential streetscape, particularly the public sidewalks and street trees, are generally in good condition. The two exceptions are Ada Avenue and Minaret Avenue where some sidewalks are missing and would be installed when these properties are redeveloped. Traffic control measures for problem locations identified by the neighborhood will be part of the Whisman Area Plan.

Streets and Sidewalks. Three major arterials serve the Ellis-Middlefield district: Whisman Road, Ellis Street, and Middlefield Road. Whisman Road north of Middlefield Road is designated as a "residential arterial" in the City's General Plan. The intersection of Ellis Street and Middlefield Road is the geographic center of the district and a major focal point. There are no existing median islands on these arterials, with the exception of a median on Middlefield Road that extends partially into the district.



ELLIS-MIDDLEFIELD INDUSTRIAL AREA

FIGURE A

Streetscape Guidelines for the Ellis-Middlefield District

In addition to these major streets, there are six other minor streets. There is a more intimate scale on these other streets, not only due to the narrower street width, but also the smaller size of the properties and buildings. These minor streets are not in a standard grid pattern, so there are large areas within the district where there are no cross-streets. This makes it more difficult for pedestrians to get from one place to another and for new visitors to get oriented. Most of the area has been developed with sidewalks immediately adjacent to the road (known as monolithic sidewalks). The combination of wide arterials with monolithic sidewalks creates an unfriendly environment for pedestrians. The following chart summarizes existing street widths and the predominant sidewalk patterns in the Ellis-Middlefield District. The width of monolithic sidewalks includes a 6" curb.

<u>STREET</u>	<u>RIGHT-OF-WAY WIDTH</u> (including sidewalk)	<u>TYPE OF SIDEWALKS</u>
Whisman Road	80'	
Residential side		5' monolithic
Industrial side		4' with parkway strip
Ellis Street	80' - 85'	5' monolithic/varies
Middlefield Road	100'	5' monolithic
Fairchild Drive	50'	5' monolithic
National Street	60' - 65'	no sidewalks/5' monolithic
Logue Avenue	70'	6' monolithic
Maude Avenue	70'	6' monolithic/some missing
Clyde Avenue	70'	5' monolithic
Ferguson Avenue	60'	5' monolithic

Railroad Lines/Future Light Rail. The Tasman Light Rail will be constructed on the old Southern Pacific rail lines that run north and south through the middle of the District. Two new stations are planned in this segment: the Middlefield Station on Middlefield Road and the Whisman Station to the south. A third station, the NASA/Bayshore Station, is located just outside the District north of Highway 101 off Ellis Street.

Gateways and Focal Points. There are two major gateways located within the Ellis Middlefield district: one at Ellis Street and Highway 101 and another at Middlefield Road and SR 237. The City's General Plan identifies these locations as important entries into Mountain View. The Ellis/Highway 101 gateway is also where the Light Rail line will enter the City. These gateway locations currently lack any distinguishing features, although CalTrans will be installing some new landscaping as part of overpass improvements to SR 237 and the ramp realignment for Light Rail at Ellis Street. Distinct features and improvements at these gateways would create an identifiable entry to the City and a stronger boundary for the Ellis-Middlefield district.

There are also two major focal points: one at the intersection of Whisman and Middlefield Roads and another at the T-intersection of Ellis Street and Middlefield Road. The Whisman-

Streetscape Guidelines for the Ellis-Middlefield District

Middlefield intersection is the geographic center of the entire Whisman Area. It marks the border where the residential and industrial districts meet and is an important entry to each of these districts. The Ellis-Middlefield focal point is the center of the industrial district. It is located next to the future Middlefield Light Rail Station and will become a hub of increased pedestrian activity. Significant private redevelopment is taking place on both the northeast and northwest corners of this intersection and on the opposite side of Middlefield Road there is already a distinctive plaza fountain.

Street Trees. The City maintains a Master Street Tree List for all streets in Mountain View. Approved street trees in the Ellis-Middlefield District are shown below.

<u>STREET</u>	<u>STREET TREE</u>
Whisman Road	Fringe Tree, London Plane Tree
Ellis Street	Tulip Tree
Middlefield Road	Maidenhair, Raywood Ash
Fairchild Drive	Sweet Gum
National Street	Schwederi Maple
Logue Avenue	Golden Rain Tree
Maude Avenue	Scarlet Oak
Clyde Avenue	Schwedleri Maple
Ferguson Avenue	Scarlet Oak

Predominant Private Landscaping. Clusters of dense evergreen trees predominate in the Ellis-Middlefield District, specifically Redwoods, Pines, Cedar, and Evergreen Elm. Newer development uses groves of these evergreen trees along with mounded lawn in the setback area. Many varieties of deciduous accent trees and broadleaf evergreen trees are also planted in the setback, including Birch, Magnolia, Liquidambar, Liriodendron, and Ash. Few of these trees are used with any consistency and the streetscape has a visually eclectic appearance.

Streetscape Guidelines for the Ellis-Middlefield District

A. GENERAL GUIDELINES

1. Landscaping within the District should be a rich blending of three major elements: 1) a strong forested background of tall redwoods and other conifers, 2) accents and groupings of deciduous or flowering trees that add color and a softer form, and 3) linear arrangements of canopy trees that define pedestrian walkways.
2. Lawn or very low, durable groundcover should be used in parkway areas between the curb and sidewalk and should be blended with the private landscaping. Lawn should be water conserving varieties such as dwarf fescue.
3. Street trees should be planted approximately 30'-45' on center depending on the variety of tree and the location of City utilities, fire hydrants, driveways and similar features. Missing trees should be replaced with varieties listed on the City's Master Street Tree List.
4. All public and private landscaping should be chosen, designed, and maintained to conserve water and include water conserving plants.
5. All streets shall have similar street trees on both sides, except Whisman Road where the following guidelines shall apply.
 - a. Fringe Trees (*Chionanthus retusus*) should be used as the street tree on the residential side of Whisman to complement the intimate scale of homes on this side. These trees should be planted approximately 30' on center in back of the sidewalk at the option of the property owner.
 - b. London Plane Trees (*Platanus acerifolia* 'Columbia') should be used as the street tree on the industrial side of Whisman to provide tall, broad canopy trees more compatible with the scale of industrial buildings. These trees should be planted in a new 7' parkway strip, approximately 30' on center.
6. Plant selection within the Ellis-Middlefield industrial district should conform to the plant palettes in Appendix A.
7. Landscaping plans on public and private property should incorporate a maintenance plan that includes a watering schedule to ensure that irrigation is appropriately zoned for native and drought tolerant plants.

Streetscape Guidelines for the Ellis-Middlefield District

B. MEDIANS

1. GENERAL GUIDELINES FOR MEDIANS

- a. Trees should be planted at least 6' from the face of the median curb and not within 50' of the end of the median island to preserve safe sight distance. These are general guidelines that may be modified to take into consideration variable factors, such as the speed of traffic and the street width and surrounding land uses.
- b. Tree canopies that will hang over the roadway should provide a minimum 14' vertical clearance from street level above travel lanes and 8' clearance in parking lots.
- c. Trees should be at least 5' from utility mains and 10' from manholes and gate valves.
- d. All shrubs shall be low growing and hardy varieties to avoid bare spots and maintain visibility across the median.
- e. Medians shall incorporate features for the safety of maintenance workers, including below ground irrigation systems to reduce the risk of tripping and 12" paving along the curbs (18" total including curbs) to provide a buffer area for maintenance crews to work.

- f. All median island noses and medians less than five-feet wide should be paved with decorative paving rather than landscaping to maintain good turning visibility and eliminate the need for frequent maintenance.

2. WHISMAN ROAD MEDIANS

a. North of Middlefield (Figure B)

- 1) The medians should be approximately 18' in width, as specified in the Whisman Traffic Plan.
- 2) Landscaping should include low, gentle mounding to create a strong buffer between the residential neighborhood and industrial businesses.
- 3) Trees should be evergreen varieties in informal groupings (20' on center) mixed with groundcover and shrubs as listed in Appendix A.

b. South of Middlefield Road (Figure C)

- 1) The medians should be 8' - 10' in width, as specified in the Whisman Traffic Plan.
- 2) Landscaping should have a transparent quality and no mounding to maintain a connection between

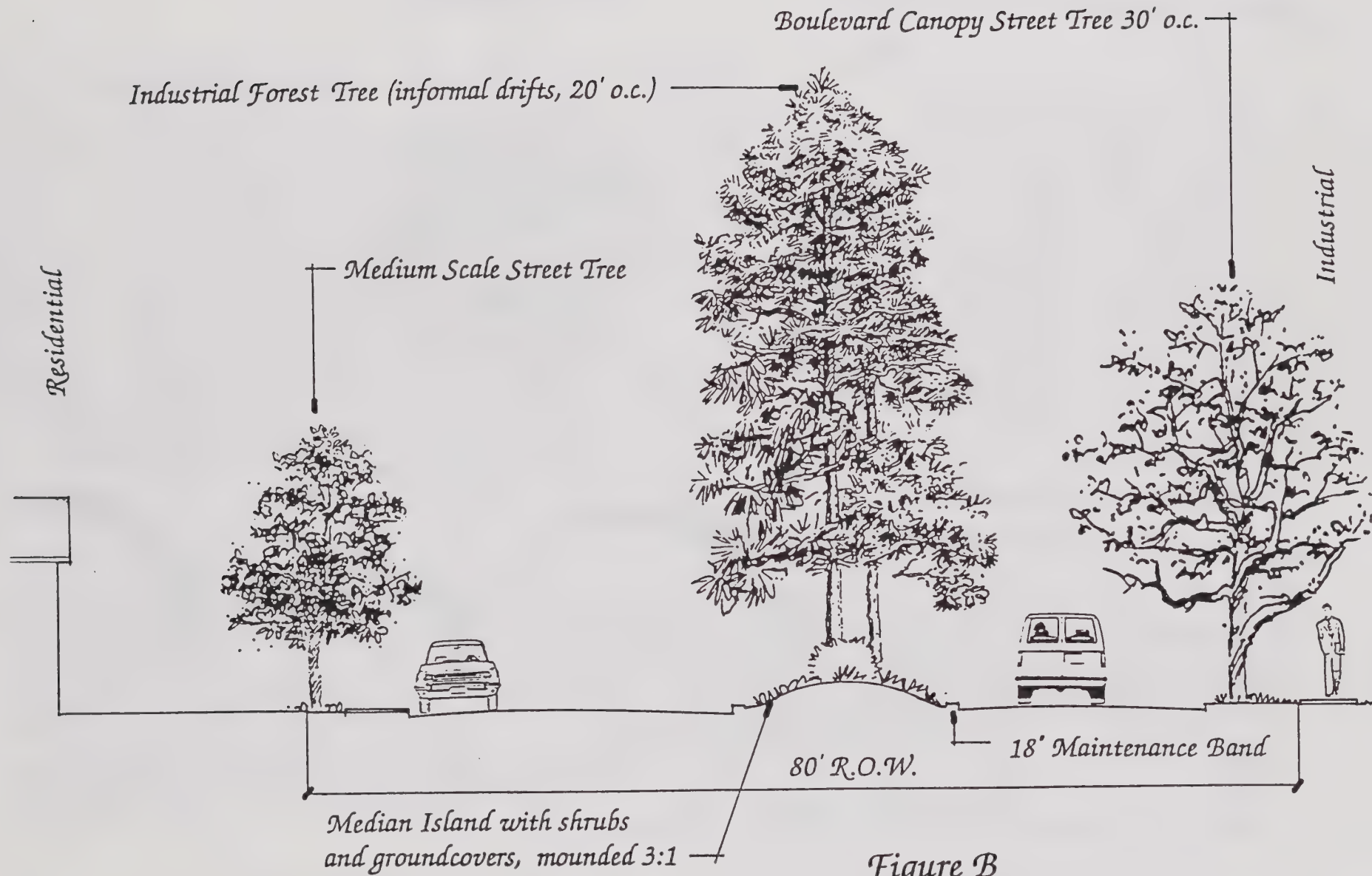


Figure B

*Whisman Road Median Island
(North of Middlefield Road)*

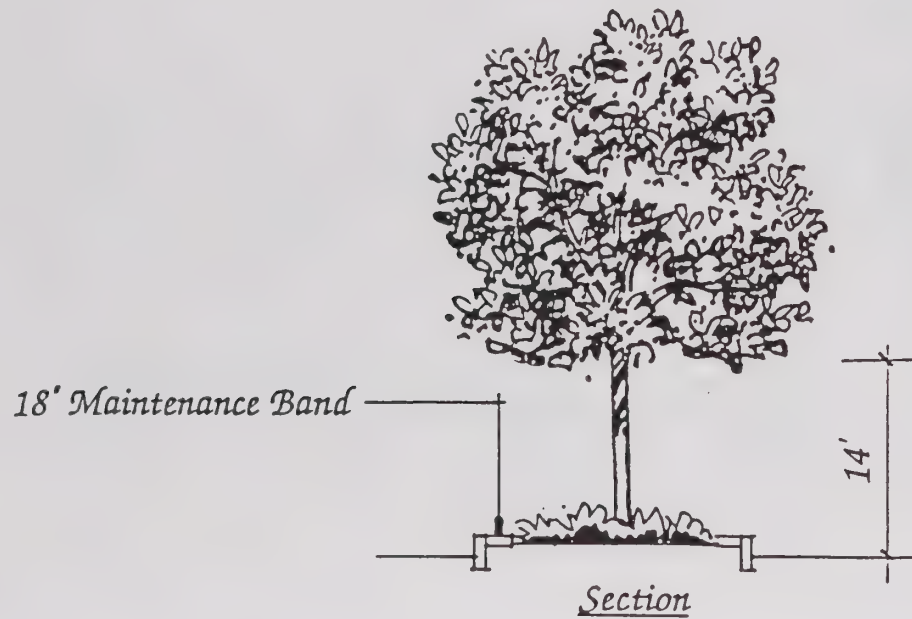
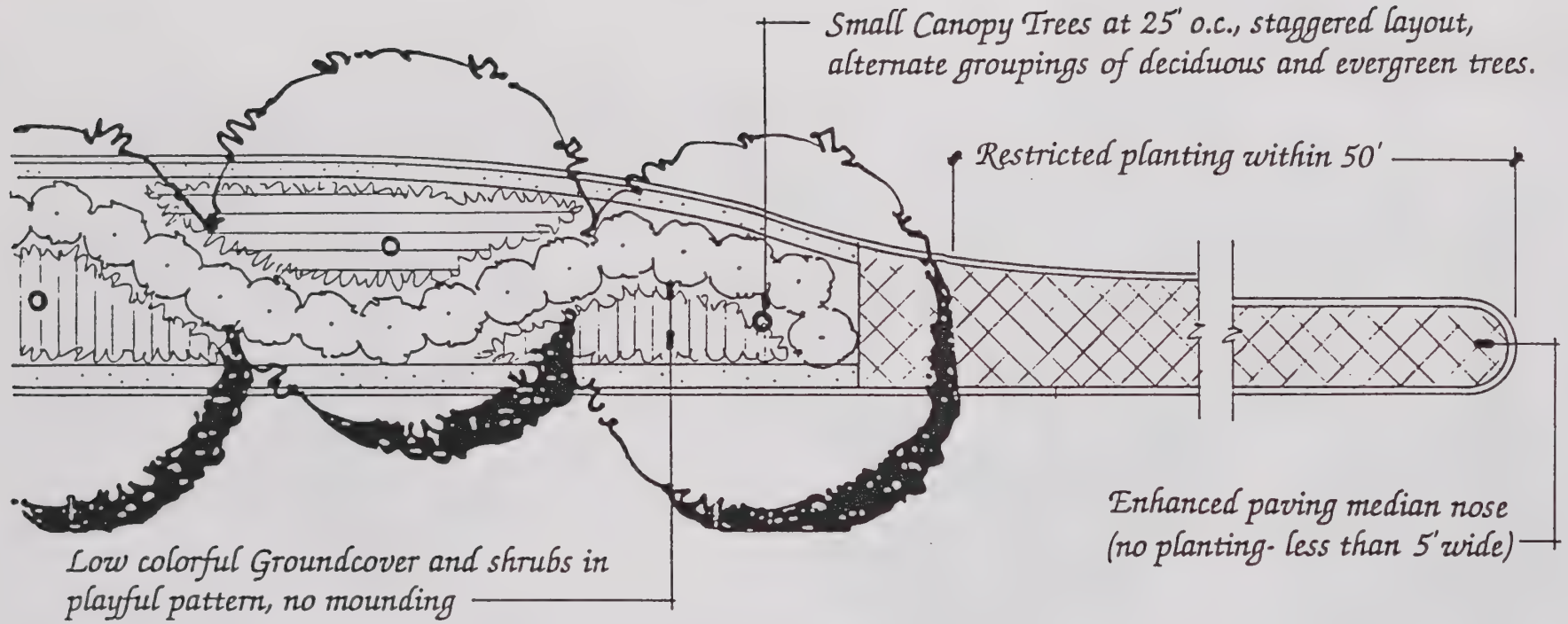


Figure C
Whisman Road Median Island
(South of Middlefield Road)

Streetscape Guidelines for the Ellis-Middlefield District

residential uses on both sides of the street.

features at this intersection (as shown in Figure G).

- 3) Trees should be small canopy trees (25' on center) that are staggered in alternate groups of evergreen and deciduous trees as listed in Appendix A (Plant Palettes).

- 4) Low ground cover and dwarf shrubs should be used to maintain a high degree of visibility across the median.

3. MIDDLEFIELD ROAD (Figure D)

- a. The medians should be about 16' in width.
- b. Landscaping for this segment of Middlefield Road should continue the landscaping pattern of the existing medians on Middlefield to create a sense of continuity along this major arterial, as indicated in Appendix A (Plant Palettes).
- c. Trees should be sparsely grouped and have non-invasive root systems to minimize the possibility of root damage to utility mains. The cost and feasibility of using root barriers for protection or dog-legging the lines around tree groupings should be considered when the capital improvement plan is developed for the medians.
- d. The noses of the Middlefield medians at Whisman Road should include a row of Crape Myrtles with uplighting as part of the focal point

4. ELLIS STREET

- a. The feasibility of median islands on the north and south ends of Ellis Street should be studied. A median island on the north end is particularly important to provide an opportunity for gateway features at this location.
- b. Landscaping on the Ellis Street medians should be similar to the medians on Middlefield Road with flat topography, boulders, and spaced tree groupings.

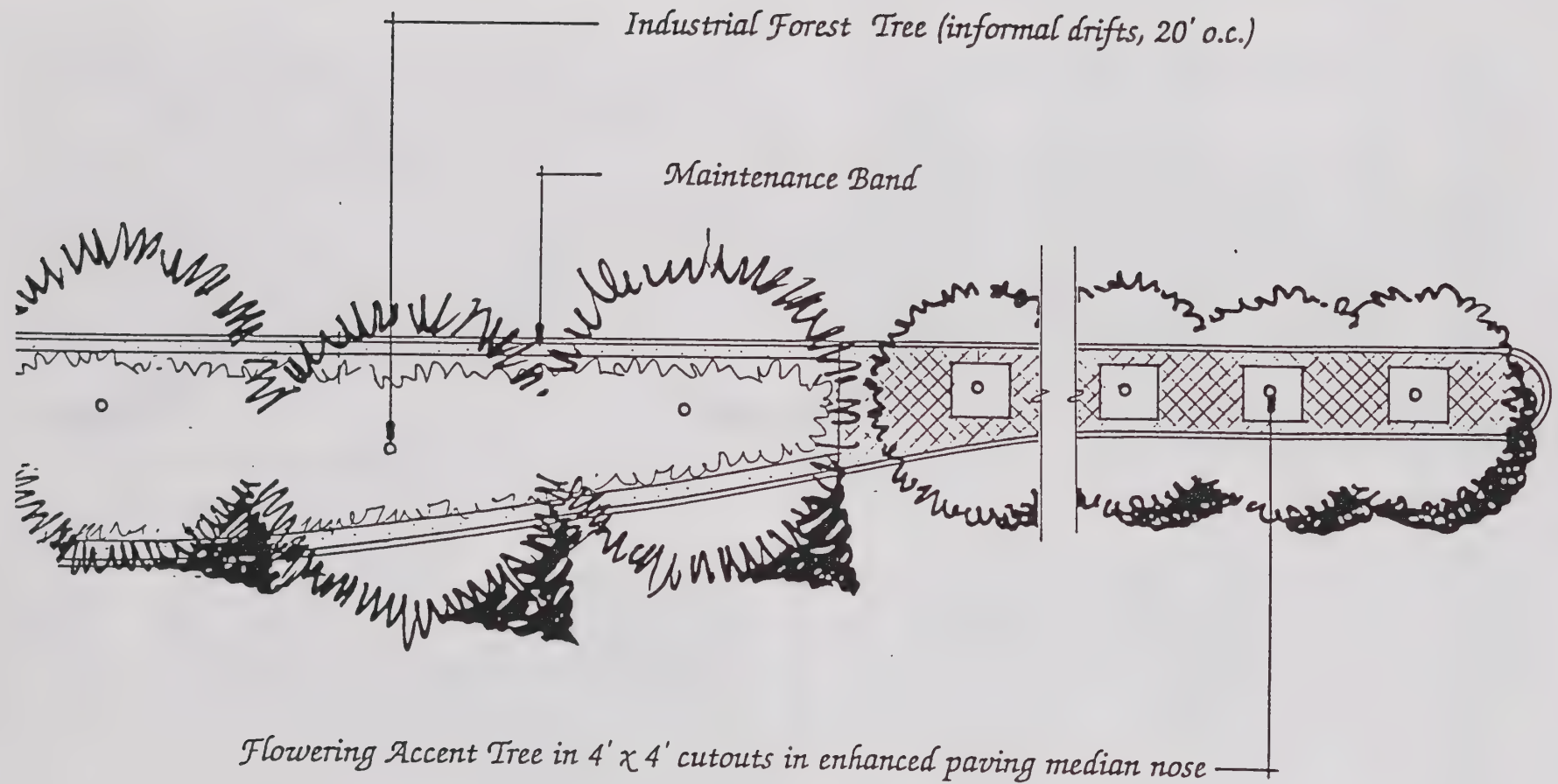


Figure D

Ellis Street & Middlefield Road Median Islands

Streetscape Guidelines for the Ellis-Middlefield District

C. GATEWAYS AND FOCAL POINTS

1. GATEWAYS (Figure E)

- a. Gateway features at Ellis Street/ Highway 101 and at Middlefield Road/State Route 237 should be incorporated into new median islands in these locations and should include the following features.

- 1) A convex, three -foot high wall as shown in Figure E.
- 2) Low shrubs and ground cover in front of the wall that include plants listed in Appendix A (Plant Palettes)
- 3) A Coast Live Oak Tree (*Quercus agrifolia*) as the signature tree planted immediately behind the wall and framed in the background by Chinese Pistache Trees (*Pistacia chinensis*).

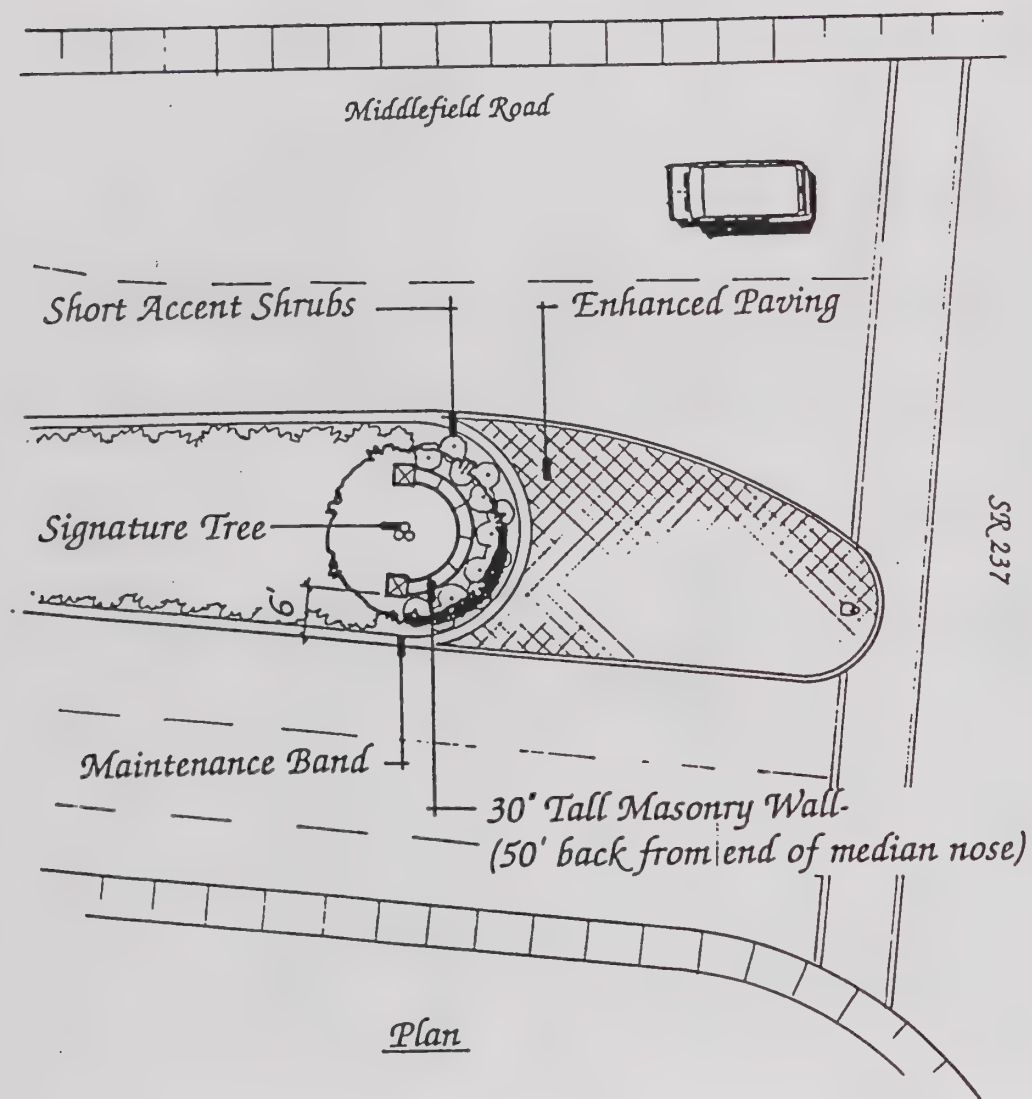
2. ELLIS-MIDDLEFIELD FOCAL POINT (Figure F)

- a. The focal point at Ellis Street and Middlefield Road should incorporate similar kinds of features as the gateways. These features should be installed on private properties on the two corners and the property at the apex of Ellis Street (located on

Middlefield Road). There should be a high degree of symmetry on the two corners. Each corner should mirror the other to mutually create a public space that is clearly part of the pedestrian environment. The property at the apex of Ellis Street (which already has a major fountain feature) may incorporate features that are different from the corners, but should contain enough of the same elements to complete the sense that this is the hub of the District. The corner locations should incorporate the following features.

- 1) A convex wall that matches the wall design for the gateways as shown in Appendix B (Details).
- 2) A “plaza” area in front of the wall that includes a seating area, artwork, decorative pavement, tree wells, large pots of perennials and annuals for seasonal color, and trash containers.
- 3) A Coast Live Oak Tree (*Quercus agrifolia*) as the signature tree planted immediately behind the wall. Ornamental Pear Trees (*Pyrus calleryana* ‘Chanticleer’) should be planted in tree wells in the plaza areas.

E-14



SR 237



Wall Elevation

Figure E
Gateway Theme

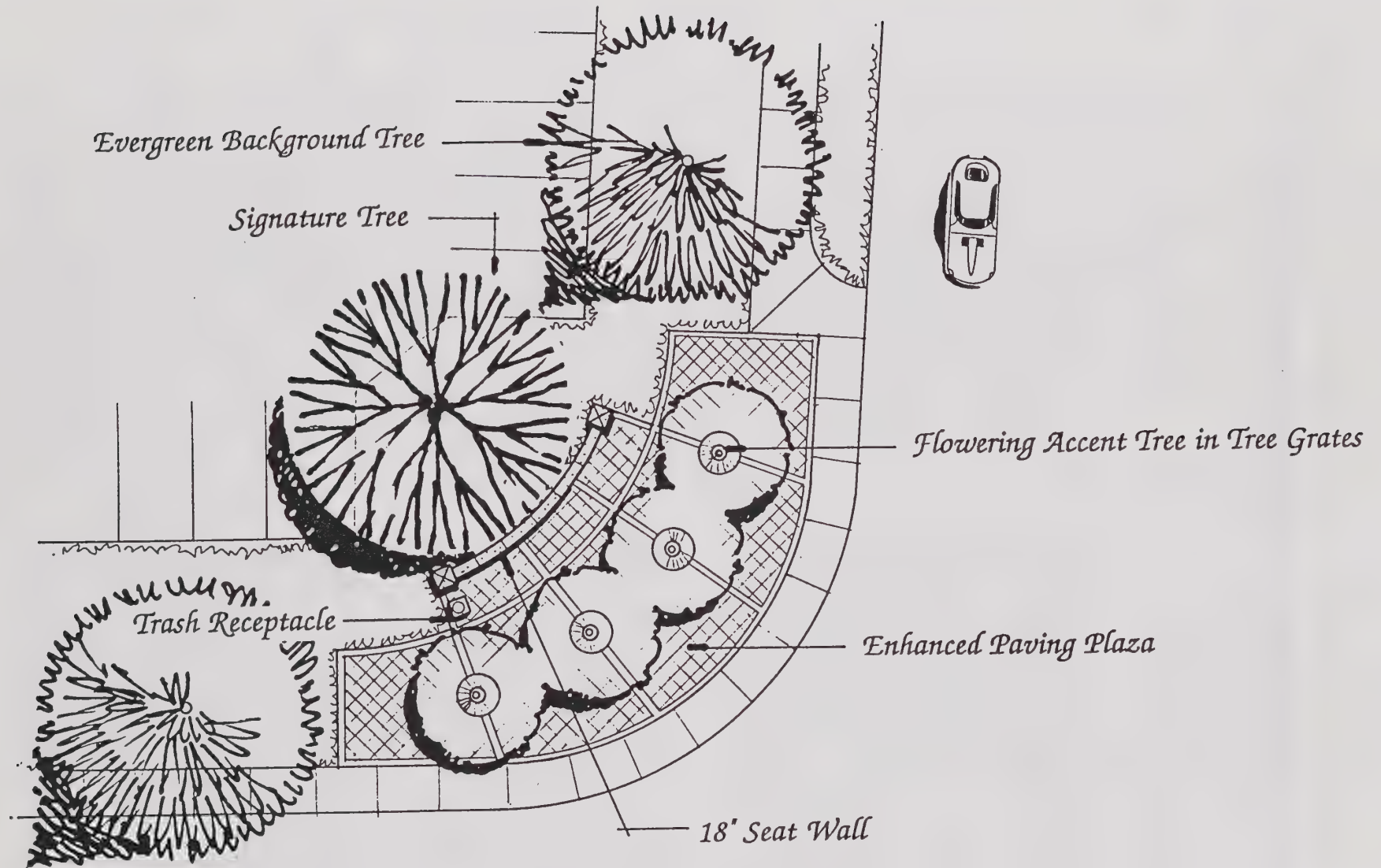


Figure F
Ellis-Middlefield
Focal Point

Streetscape Guidelines for the Ellis-Middlefield District

3. WHISMAN-MIDDLEFIELD FOCAL POINT (Figure G)

- a. The focal point at the intersection of Whisman and Middlefield Roads should incorporate features that identify it as the center of the Whisman area and entries to the residential and industrial districts. Private landscaping and public improvements should create design continuity on the four corners and entry features on Middlefield Road that enhance this intersection and the Whisman Area. The focal point should include the following features, as shown in Figure G.
 - 1) Similar landscaping on each corner that includes a gently bermed lawn area with a convex back border defined by a clipped three foot hedge of *Escallonia* 'Compakta' on the commercial side and a similar hedge with a decorative wall in the middle on the industrial side. The decorative wall should be the same color and general design as the seat walls for the Ellis-Middlefield focal point, except that it can have a narrower width and not be designed for seating. *Agapanthus* 'Peter Pan' should be planted at the corners of the hedges to add color to this landscape feature.
 - 2) Textured and colored sidewalk pavement on all four corners extending to the back of the hedge.
 - 3) Crape Myrtle (*Lagerstroemia indica*) should be used as the accent tree in the nose of the medians on both sides of Middlefield Road. Up-lighting should be installed to accent these trees and provide as sense of entry.
 - 4) Further study should be undertaken to determine the feasibility of the following features and improvements:
 - a) Metal entry signs added to existing traffic signal poles to identify both the industrial and residential districts. Entry signs should be studied as part of a City-wide program to identify districts and neighborhoods and should involve the participation of businesses and residents in the Whisman Area.
 - b) Undergrounding the overhead utilities that pass through the intersection on the east side of Whisman Road.

Streetscape Guidelines for the Ellis-Middlefield District

D. TASMAN LIGHT RAIL

1. STATION AREAS

- a. Landscaping around the Whisman and Middlefield Light Rail Stations should be a comfortable environment for pedestrians and include the following elements: extensive paved areas so people can easily walk to the station from many different directions and in all kinds of weather, seating for comfortable waiting areas, canopy trees for shade, lighting and trash containers to minimize litter.
- b. Tree wells should be used in paved areas to provide tree canopies over plazas and pathways, improve pedestrian circulation, and introduce an urban design vocabulary similar to the Downtown Mountain View Light Rail Station.
- c. The predominant tree near Light Rail Stations should be Paperbark Maple (*Acer griseum*).
- d. Pavement around Light Rail Stations should include decorative elements and colors that reduce glare and add interesting patterns.
- e. Seating should be located in protected, shady areas.
- f. Fencing at the Light Rail Stations along the common property line with private property shall be black

tubular steel, 5'-2" high as shown in Appendix B (Details).

2. LIGHT RAIL "FRONT YARDS" (Figures H and I)

- a. Buildings fronting the Light Rail lines should incorporate a "front door" and other architectural refinements on the rear elevation to enhance the new public "street" that Light Rail will bring into the district.
- b. Buildings should be designed to be highly public next to Light Rail Stations and incorporate features that maintain a human scale.
- c. A clearly identified sidewalk should be installed from the main entry on the rear elevation to proposed Light Rail pedestrian pathways along the rear property line.
- d. Properties next to the Light Rail line should provide a minimum 6' landscaping along the rear property line that includes London Plane Trees (*Platanus acerifolia* 'Columbia').
- e. All outdoor storage should be fully screened with masonry walls designed to complement the building colors and design.

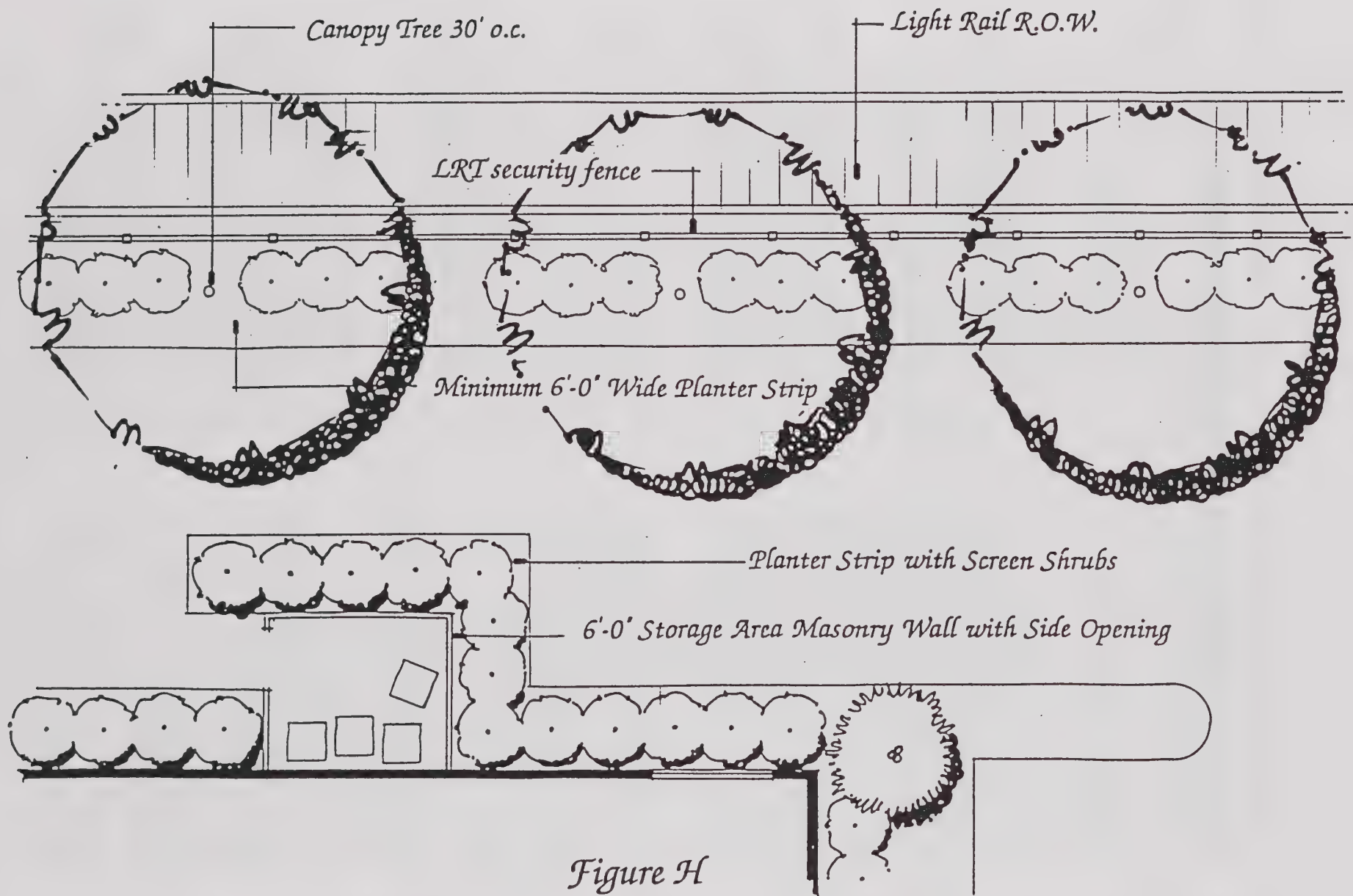


Figure H
Light Rail Improvement on Private Property

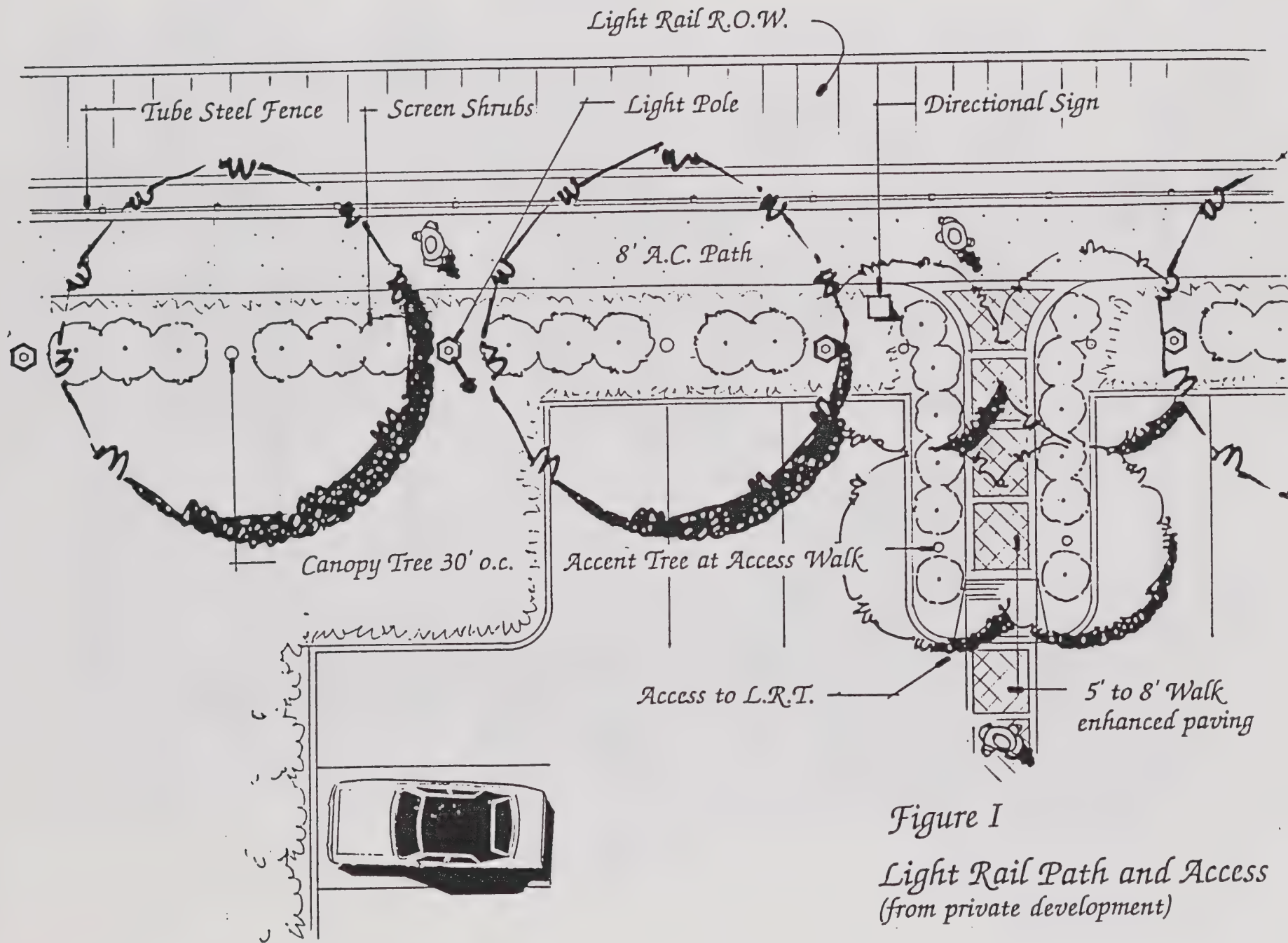


Figure I
Light Rail Path and Access
(from private development)

Streetscape Guidelines for the Ellis-Middlefield District

E. PEDESTRIAN PATHWAYS

1. PUBLIC SIDEWALKS

- a. Sidewalks should be improved to meet the following standards when properties are redeveloped, buildings remodeled, or there are major landscaping or site modifications. Where sidewalk widening is required, the entire sidewalk should be replaced rather than adding pavement to an existing sidewalk.
 - 1) On Whisman Road (east side) and Ellis Street a 7' wide parkway strip and 6' wide detached sidewalk should be installed.
 - 2) On Middlefield Road a 6' wide monolithic sidewalk should be installed.
 - 3) On Logue Avenue and Maude Avenue a 6' wide monolithic sidewalk should be installed.
 - 4) On Fairchild, National Avenue, Clyde Avenue, and Ferguson Drive: a 5' wide monolithic sidewalk should be installed.
- b. When monolithic sidewalks are reconfigured to accommodate a landscape parkway, traffic signal poles may also need to be moved and the cost included in improvement plans.

- c. When sidewalk improvements would result in the loss of mature trees, the sidewalk may meander, jog, or vary in width in order to preserve the trees.

2. LIGHT RAIL PATHS (Figure J)

- a. The location of proposed pedestrian pathways to the Middlefield and Whisman Light Rail Station is shown in Figure A.
- b. Copper Beech Trees (*Fagus Sylvatica* 'Purpurea') planted at 25' on center should be the signature trees planted along pedestrian paths to the light rail. The size and distinctive color of these trees will help make the path easily identifiable and provide shade for pedestrians.
- c. Windmill Palms (*Trachycarpus fortunei*) should be planted to visually accentuate the entrances to the pedestrian pathways.
- d. Lighting standards should be installed at consistent intervals to provide even lighting. A post top design should be used to create a more human scale and distinguish the trail as shown in Appendix B (Details).



Figure 1

Streetscape Guidelines for the Ellis-Middlefield District

- e. Pavement should be a consistent material for pathways on private properties and include features that define the entries to the path from the street. Special paving should be used where paths cross parking lots to differentiate the path.
 - f. Low directional signs should be installed where light rail pathways cross roadways to assist pedestrian in finding the easiest access to Light Rail Stations. These signs should all have a similar size, shape, color, and lettering.
 - g. The Light Rail walkway width should be 8' in width and wherever possible should be bordered with at least 5' of landscaping on one side.
3. PEDESTRIAN CONNECTIONS ON WHISMAN ROAD (Figure K)
- a. The pedestrian connection at Whisman Road between the Hetch-Hetchy/ Stevens Creek Trail and pedestrian/bicycle pathways to the Light Rail should incorporate the following features.
 - 1) Low directional signage should be installed at the entrance of pathways to the Light Rail at Whisman Road to provide pedestrians with directions and similar signage should be installed on the opposite side to direct pedestrians to the Stevens Creek Trail.
 - 2) Special landscaping features, such as smooth granite boulders, wooden bollards and railings, and unique plant materials, should be installed at the Whisman Road entries to the paths to make them appealing and easy to identify.
 - b. Streetscape features for the pedestrian connection between new housing south of the Ellis-Middlefield industrial district and Slater Elementary School on the opposite side of Whisman Road should be oriented toward the safety of school children and include features that will provide high visibility.
 - 1) Median islands should be provided where new traffic signals will be installed at Gladys Avenue and Whisman Road to provide a safe stopping place for children crossing the street.
 - a) Within 50' of pedestrian crosswalks at this intersection only restricted landscaping such as low shrubs, should be installed.
 - b) Other landscaping within the medians near the crosswalks should be highly transparent and minimize distractions for drivers.

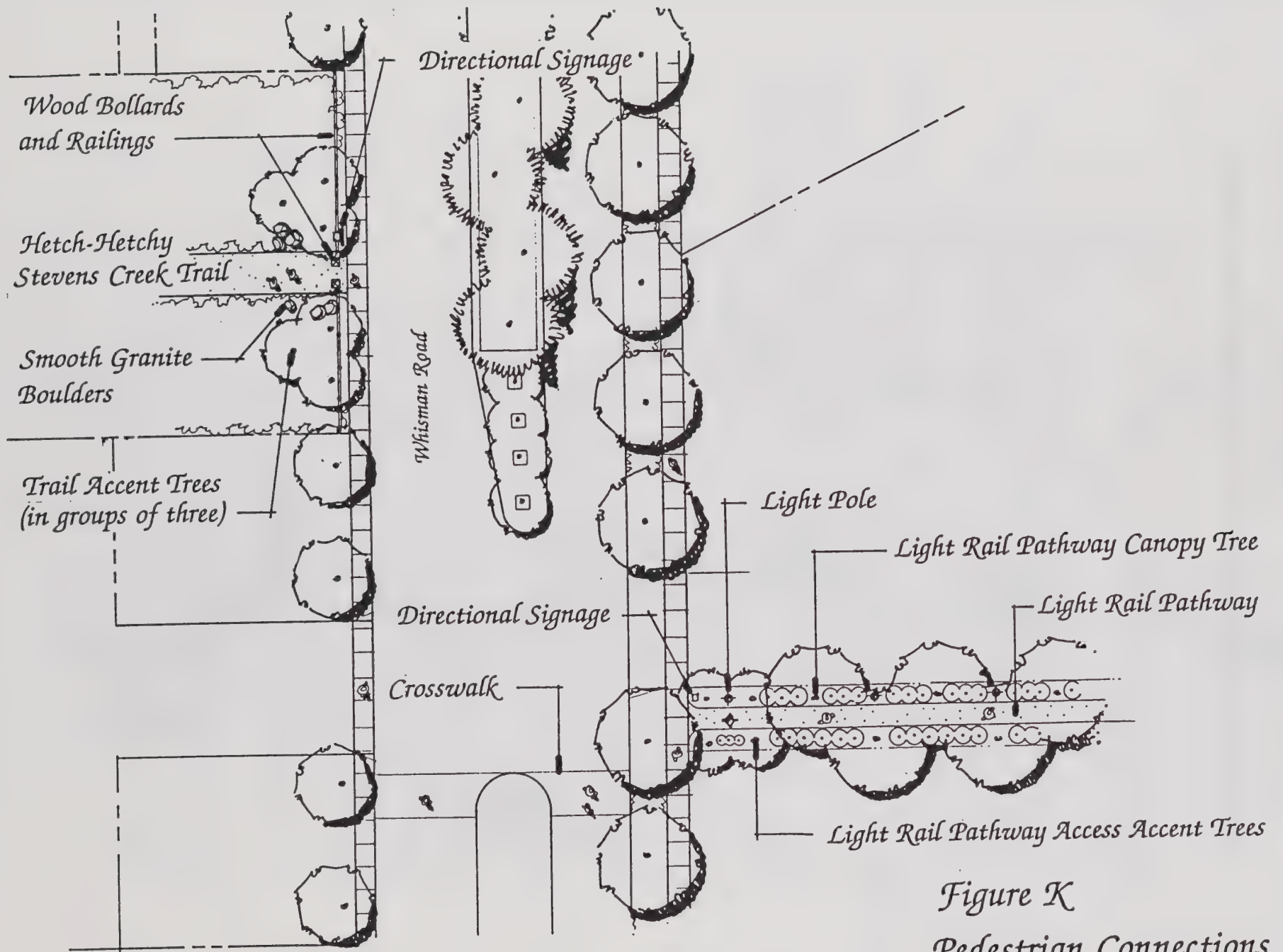


Figure K
Pedestrian Connections

Streetscape Guidelines for the Ellis-Middlefield District

F. PRIVATE LANDSCAPING

(Figures L, M, and N)

1. Private landscaping adjacent to the street should blend with landscaping in the public right-of-way to create a cohesive streetscape and a blending of the public and private areas.
2. Gentle mounding (typically 3:1 horizontal to vertical slope) should be used to screen parking lots and provide interesting landscape contours in back of the public sidewalks.
3. To increase pedestrian and vehicular safety, driveways should be consolidated and the landscaping in driveway areas should maximize visibility through the careful placement of appropriate plants, such as high canopy trees and low ground cover.
4. On major arterials, the majority of trees adjacent to the sidewalk generally should be arranged in informal groups of two or three approximately every 40' as shown in Figure K. Accent trees should be interspersed to define driveways and front entries and to provide variety.
5. Landscaping should clearly accentuate the entry to the building and include paved pathways to the entrance, so that "front doors" are visible and accessible from the street.
6. Properties adjacent to bus stops should provide amenities to encourage bus ridership, such as shade trees and seating areas.
7. Private landscaping should be well maintained, including: routine watering, mowing, trimming, and fertilizing; replacement of diseased or dying plants; and ensuring that plantings do not over-grow or create obstructions on sidewalks or other pedestrian areas.

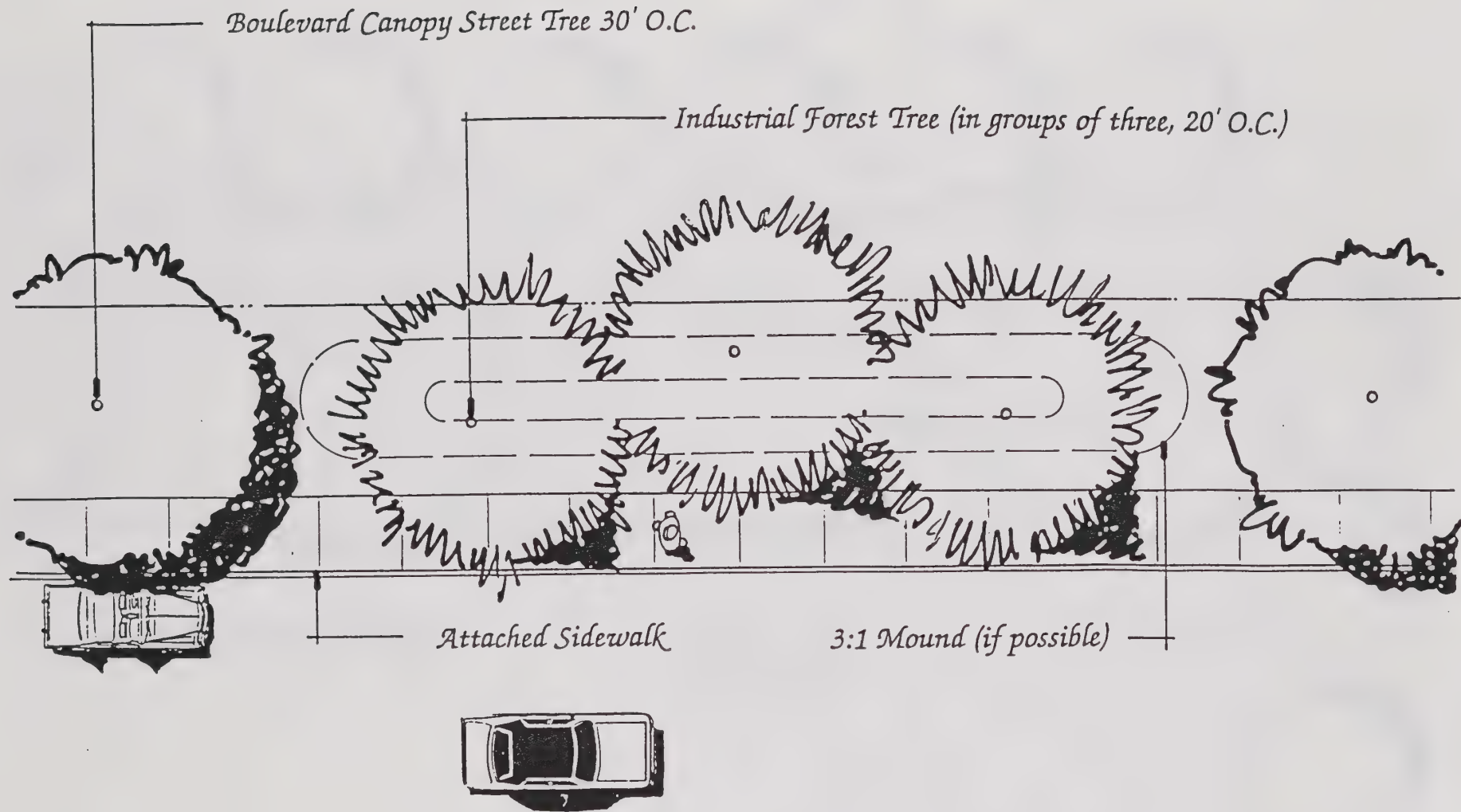


Figure L
Middlefield Road Planting
(major street, attached sidewalk)

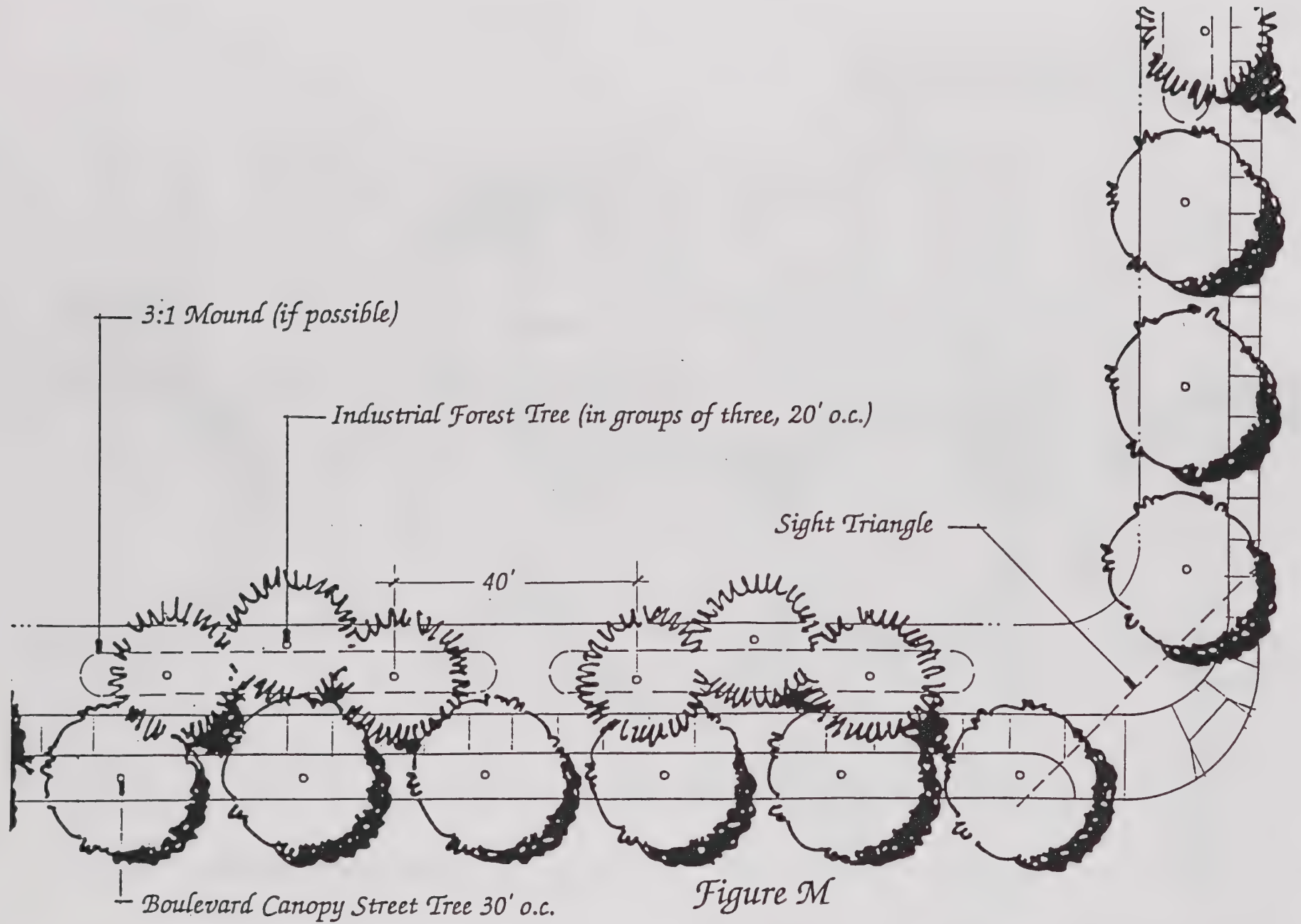


Figure M
Ellis Street & Whisman Road Planting
(detached sidewalk)

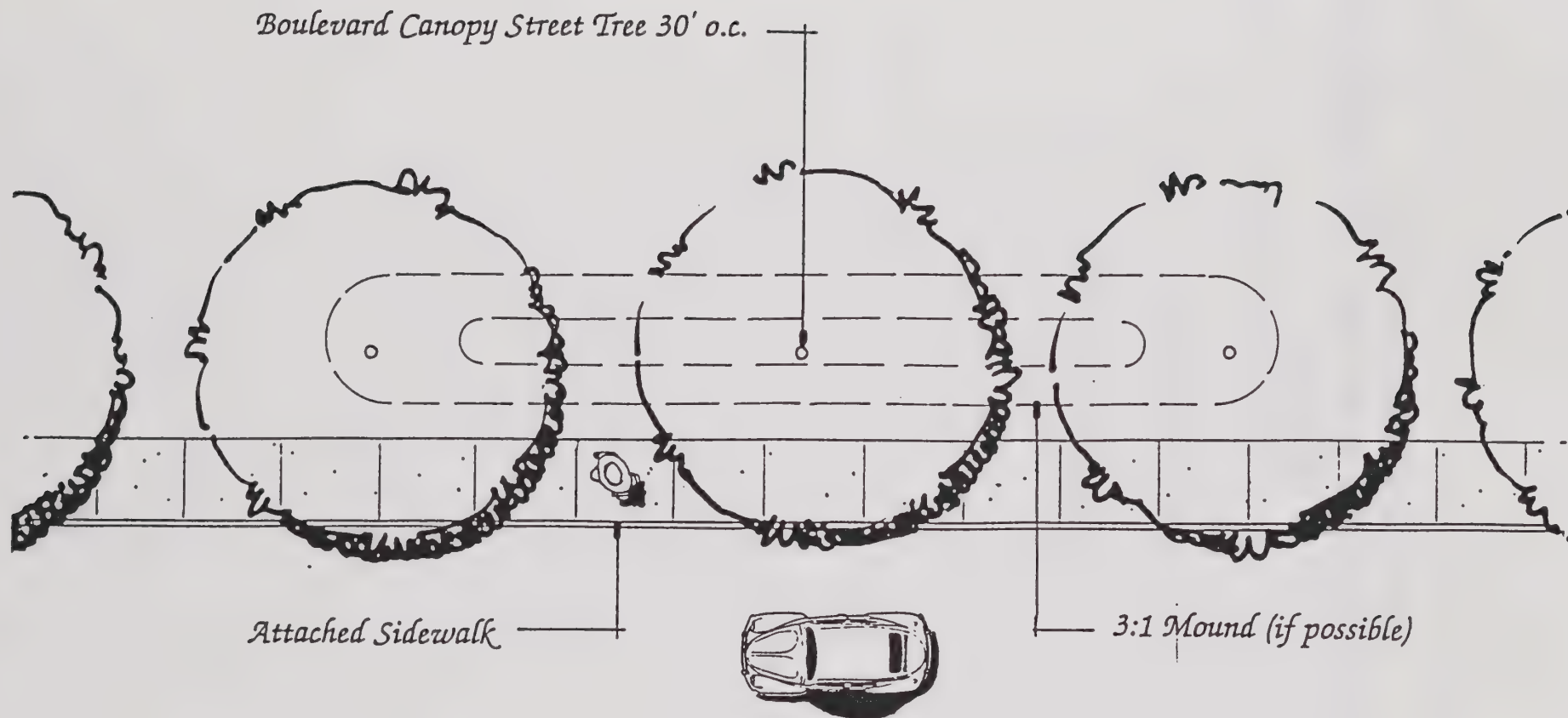


Figure N

*National Ave., Logue Ave. Maude Ave.,
Claude Ave., & Ferguson Dr. Planting
(minor street, attached sidewalk)*

**City of Mountain View
Streetscape Guidelines for the Ellis-Middlefield
District**

APPENDIX A

PLANT PALETTES

MEDIAN ISLAND PLANT PALETTE

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
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WHISMAN ROAD SOUTH OF MIDDLEFIELD

TREES

MALL SCALE DECIDUOUS CANOPY TREE

PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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MALL SCALE EVERGREEN CANOPY TREE

ARBUTUS MARINA	N.C.N.	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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SHRUBS

MEDIUM SCALE FLOWERING SHRUBS

CORREA 'CARMINE BELLS'	AUSTRALIAN FUCHSIA	1 GAL.	10"-12" X 10"-12"	
LEPTOSPERMUM SCOPARIUM 'HELENE STRYBING'	NEWZEALAND TEATREE VAR. HELENE STRYBING	5 GAL.	18"-24" X 18"-24"	
RAPHIOLEPIS INDICA 'BALLERINA'	INDIA HAWTHORN VAR. 'BALLELRINA'	5 GAL.	12" X 12"	

MALL SCALE ACCENT SHRUBS

AGAPANTHUS AFRICANUS 'PETER PAN'	LILY-OF-THE-NILE VAR. 'PETER PAN'	1 GAL.	12" X 12"	
PHORMIUM TENAX 'MAORI MAIDEN'	FLAX VAR. 'MAORI MAIDEN'	5 GAL.	18" X 18"	

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
GROUNDCOVERS				
HEMEROCALLIS FULVA	TAWNY DAYLILY	1 GAL.	@ 36" O.C.	TRIANGULAR SPACING USE IN 20-30' DRIFTS
ROSA RUGOSA 'FLOWER CARPET'	FLOWER CARPET GROUNDCOVER ROSE	1 GAL.	@ 24" O.C.	TRIANGULAR SPACING USE IN 20-30' DRIFTS
ROSMARINUS OFFI. ANGUS. 'PROSTRATUS'	DWARF ROSEMARY	1 GAL.	@ 24" O.C.	TRIANGULAR SPACING USE IN 20-30' DRIFTS

WHISMAN ROAD NORTH OF MIDDLEFIELD

TREES

LARGE SCALE EVERGREEN CONIFEROUS TREES:

PINUS CANARIENSIS	CANARY ISLAND PINE	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
SEQUOIA SEMPERVIRENS VAR. 'APTOS BLUE'	COAST REDWOOD VAR. 'APTOS BLUE'	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE

SMALL SCALE FLOWERING TREES

CERCIS CANADENSIS	EASTERN REDBUD	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE STANDARD
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SHRUBS

LARGE SCALE EVERGREEN SCREEN SHRUBS

ESCALLONIA 'TERRI'	N.C.N.	5 GAL.	12" X 12"	
HETEROMELES ARBUTIFOLIA	TOYON	5 GAL.	12" X 12"	
RAPHIOLEPIS INDICA VAR. 'SPRINGTIME'	INDIA HAWTHORN VAR. 'SPRINGTIME'	5 GAL.	12" X 18"	

MEDIUM SCALE FLOWERING SHRUBS

LEPTOSPERMUM SCOPARIUM 'HELENE STRYBING'	NEWZEALAND TEATREE VAR 'HELENE STRYBING'	5 GAL.	18"-24" X 18"-24"	
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BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
RAPHIOLEPIS INDICA	INDIA HAWTHORN	5 GAL	12" X 12"	
'BALLERINA'	VAR. 'BALLELRINA'			

MALL SCALE ACCENT SHRUBS

AGAPANTHUS AFRICANUS 'PETER PAN'	LILY-OF-THE-NILE VAR. 'PETER PAN'	1 GAL.	12" X 12"
DIETES BICOLOR	FORTNIGHT LILY	5 GAL.	18" X 18"

GROUNDCOVERS

HEMEROCALLIS FULVA	TAWNY DAYLILY	1 GAL	12" X 12"	TRIANGULAR SPACING USE IN 20'-30' DRIFTS
ROSMARINUS OFFI. ANGUS. 'PROSTRATUS'	DWARF ROSEMARY	1 GAL.@.36" O.C.		TRIANGULAR SPACING USE IN 20'-30' DRIFTS

MIDDLEFIELD ROAD AND ELLIS STREET

TREES

LARGE SCALE EVERGREEN CONIFEROUS TREES:

PINUS CANARIENSIS	CANARY ISLAND PINE	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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MALL SCALE FLOWERING TREES

PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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SHRUBS

MEDIUM SCALE SHRUBS

RAPHIOLEPIS INDICA 'CLARA'	INDIA HAWTHORN VAR. 'CLARA'	5 GAL.	12" X 12"
XYLOSMA CONGESTUM 'COMPACTA'	N.C.N.	5 GAL.	18" X 18"

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
<u>SMALL SCALE ACCENT SHRUBS</u>				
HEMEROCALLIS FULVA	TAWNY DAYLILY	1 GAL	12" X 12"	
DIETES BICOLOR	FORTNIGHT LILY	5 GAL.	18" X 18"	
RIBES VIBURNIFOIUM	CATALINA PERFUME	1 GAL.	12" X 12"	

GATEWAY AND FOCAL POINT PLANT PALETTE

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
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TREES

GATEWAY, FOCAL POINT AND SIGNATURE TREE:

QUERCUS AGRIFOLIA	COAST LIVE OAK	36" BOX	9'-11' X 6'-7' X 2" CALIPER	MULTI TRUNK
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GATEWAY LARGE SCALE DECIDUOUS BACKGROUND TREE

PISTACHIA CHINENSIS	CHINESE PISTACHE	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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ELLIS-MIDDLEFIELD FOCAL POINT PLAZA TREE

PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE
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WHISMAN-MIDDLEFIELD FOCAL POINT TREE

LAGESTROEMIA INDICA 'TUSCARORA'	CRAPE MYRTLE VAR. 'TUSCARORA'	36" BOX	12'-14' X 3'-4' X 2" CALIPER	DOUBLE STAKE
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SHRUBS

WHISMAN-MIDDLEFIELD FOCAL POINT HEDGE SHRUB

ESCALLONIA 'COMPAKTA'	ESCALLONIA VAR. 'COMPAKTA'	5 GAL	12" X 12	
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MEDIUM SCALE FLOWERING SHRUBS

RAPHIOLEPIS INDICA 'BALLERINA'	INDIA HAWTHORN VAR. 'BALLELRINA'	5 GAL	12" X 12"	
RAPHIOLEPIS INDICA 'CLARA'	INDIA HAWTHORN VAR. 'CLARA'	5 GAL.	12" X 12"	

SMALL SCALE ACCENT SHRUBS

AGAPANTHUS AFRICANUS 'PETER PAN'	LILY-OF-THE-NILE VAR. 'PETER PAN'	1 GAL	12" X 12"	USE IN FRONT OF WALL
DIETES BICOLOR	FORTNIGHT LILY	5 GAL.	18" X 18"	

OTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
ROUNDCOVERS				
HEMEROCALLIS FULVA	TAWNY DAYLILY	1 GAL	12" X 12"	TRIANGULAR SPACING USE IN 20'-30' DRIFTS
LANTANA MONTEVIDENSIS	TRAILING LANTANA	1 GAL	@ 12" O.C.	TRIANGULAR SPACING USE IN 20'-30' DRIFTS
ROSMARINUS OFFI. ANGUS. 'PROSTRATUS'	DWARF ROSEMARY	1 GAL.	@ 36" O.C.	TRIANGULAR SPACING USE IN 20'-30' DRIFTS

LIGHT RAIL TRANSIT PEDESTRIAN PATH PLANT PALETTE

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
TREES				
<u>Signature Canopy Tree at Stations</u>				
ACER GRISEUM	PAPERBARK MAPLE	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE
<u>Signature Canopy Tree at Walks</u>				
FAGUS SYLVATICA 'PURPUREA'	PURPLE BEECH	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE
<u>Country Accent Tree</u>				
TRACHYCARPUS FORTUNEI	WINDMILL PALM	6' BROWN TRUNK		MATCHING IN GROUPS OF THREE
SHRUBS				
<u>Large Scale Evergreen Screen Shrubs</u>				
ESCALLONIA EXONIENSIS 'FRADES'	N.C.N.	5 GAL.	12" X 12"	
PHOTINIA FRASERI	RED-LEAF PHOTINIA	5 GAL.	12" X 18"	
PITTOSPORUM EUGENIOIDES	TARATA PITTOSPORUM	5 GAL.	18" X 18"	
XYLOSMA CONGESTUM	N.C.N.	5 GAL.	18" X 18"	
<u>Medium Scale Flowering Shrubs</u>				
PITTOSPORUM TOBIRA 'VARIEGATA'	TOBIRA			
RAPHIOLEPIS INDICA 'BALLERINA' VAR. 'BALLELRINA'	INDIA HAWTHORN	5 GAL.	12" X 12"	
RAPHIOLEPIS INDICA 'CLARA'	INDIA HAWTHORN VAR. 'CLARA'	5 GAL.	12" X 12"	

Botanic Name	Common Name	Size	Height x Spread	Remarks
Small Scale Accent Shrubs				
Agapanthus africanus 'Peter Pan'	Lily-of-the-Nile var. 'Peter Pan'	1 gal.	12" x 12"	
Dietes bicolor	Fortnight Lily	5 gal.	18" x 18"	
Nandina domestica 'compacta'	Heavenly Bamboo	5 gal.	12" x 12"	
Groundcovers				
Gazania rigens 'Clear White'	Gazania var. 'Clear White'	flats	@ 12" O.C.	Triangular Spacing
Rosmarinus offi. angus. 'protratus'	Dwarf Rosemary	1 gal.	@ 36" O.C.	Triangular Spacing
Vines				
Jasminum polyanthum	Pink Jasmine	5 gallon	36"-42" x 18"-24"	Vine (remove nurser
Macfadyena unguis-cati	Cats Claw Vine	5 gal.	min. 3 18" runners per plant	

HETCH HETCHY RIGHT-OF-WAY PLANT PALETTE

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
TREES				
<u>SMALL SCALE DECIDUOUS CANOPY TREE</u>				
CERCIS OCCIDENTALIS	WESTERN REDBUD	24" BOX	8'-10' X 3'-4' X 1 1/2" CALIPER	MULTI TRUNK
SHRUBS				
<u>MEDIUM SCALE FLOWERING SHRUBS</u>				
CEANOTHUS 'JULIA PHELPS'	WILD LILAC	5 GAL.	18"-24" X 18"-24"	
CISTUS 'DORIS HIBBERSON'	ROCKROSE VAR. 'DORIS HIBBERSON'	5 GAL.	12" X 12"	
RHAMNUS CALIFORNICA	COFFEEBERRY	1 GAL.	12" X 12"	
<u>SMALL SCALE ACCENT SHRUBS</u>				
IRIS DOUGLAISANA	DOUGLAS IRIS	1 GAL	12" X 12"	
CEANOTHUS GRISSEUS HORIZONTALIS 'YANKEE POINT'	CARMEL CREEPER VAR. 'YANKEE POINT'	1 GAL.	@ 12" O.C.	TRIANGULAR SPACING

PRIVATE SETBACK PLANT PALETTE

BOTANIC NAME	COMMON NAME	SIZE	HEIGHT X SPREAD	REMARKS
TREES				
LARGE SCALE EVERGREEN CONIFEROUS TREES:				
CEDRUS DEODARA	DEODAR CEDAR	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
PINUS CANARIENSIS	CANARY ISLALND PINE	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
SEQUOIA SEMPERVIRENS VAR. 'APTOS BLUE'	COAST REDWOOD VAR. 'APTOS BLUE'	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
LARGE SCALE DECIDUOUS BOULEVARD TREE				
PLATANUS ACERIFOLIA VAR. 'COLUMBIA'	LONDON PLANE TREE 'COLUMBIA'	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
SMALL SCALE FLOWERING ACCENT TREES				
CERCIS CANADENSIS	EASTERN REDBUD	15 GAL.	7'-8' X 2'-3' X 1" CALIPER	DOUBLE STAKE
CHIONANTHUS RETUSA	CHINESE FRINGE TREE	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE
LAGERSTROEMIA FAURERI 'TUSCARORA'	CRAPE MYRTLE VAR. 'TUSCARORA'	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	MULTI TRUNK DOUBLE STAKE
MALUS 'ROBINSON'	CRABAPPLE VAR. 'ROBINSON'	15 GAL.	6'-7' X 2'-3' X 1" CALIPER	DOUBLE STAKE

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GATEWAY AND FOCAL POINT SIGNATURE TREE

Quercus agrifolia, multi trunk

Coast Live Oak is a round headed, spreading evergreen tree 20-70' in height. The spread may be greater than 70' in very old age. It has smooth, dark gray bark and very dense foliage. The leaves are rounded, hollylike 1-3 inches long, and are slightly glossy on the upper surface. With irrigation, this tree can grow to 25' in ten years.

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GATEWAY LARGE-SCALE DECIDUOUS BACKGROUND TREE

Pistachia chinensis

Chinese Pistache

Chinese Pistache is a deciduous round headed canopy tree which grows moderately to 60' with a 50' spread. Leaves have 10-16 paired leaflets 2-4 inches long by 3/4 inch wide. Foliage turns bright clear scarlet, crimson, orange and sometimes yellow in autumn. Requires little water once established.

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FOCAL POINT PLAZA TREE

Pyrus calleryana 'Chanticleer'

Chanticleer Pear

Chanticleer Pear is a deciduous tree which is narrow but not columnar. It grows to a height of 40' with a 15' spread. It has a horizontal branching pattern and a strongly pyramidal shape. Leaves are deep green and very glossy, 3 inches long, broadly oval and leathery. In early spring, the tree is covered with clusters of pure white flowers. In autumn, the leaves turn bright scarlet. Requires average water to become established.

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FOCAL POINT TREE AT WHISMAN AND MIDDLEFIELD

Lagerstroemia indica 'Tuscarora'

Crape Myrtle var. 'Tuscarora'

Tuscarora Crape Myrtle is a deciduous tree developed by the National Arboretum in Washington DC. It grows moderately to 35' with a 20' spread. It is a vase shaped, upright tree, with light green spring foliage tinged bronze red. Fall foliage turns carmine, yellow to orange-red. The mature leaves are a glossy green. In mid summer, crinkled, crepe like flowers in subtle shades of pink and coral appear in dense, slightly conical clusters and can last for two months. Requires little water once established.

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SIGNATURE CANOPY TREE AT LRT WALKS

Fagus sylvatica 'Atropunicea'

Copper Beech, Purple Beech

Copper Beech is a deciduous tree which can grow to 90 feet, although usually much shorter. The tree has a broad cone shape, with smooth gray bark and reddish purple leaves which gradually turn green in summer. The leaves turn brown in winter. Needs normal watering to become established.

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ENTRY ACCENT TREE AT LRT PEDESTRIAN PATH

Trachycarpus fortunei

Windmill Palm

The Windmill Palm is a moderate to fast growing palm which can grow to 30'. Hardy to 10° F or lower, it has a dark slender trunk which is usually thicker at the top than at the base. The fronds are 3 feet across on toothed, 1 1/2 foot stalks. It responds well to regular irrigation and fertilizer.

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SIGNATURE CANOPY TREE AT LRT STATIONS

Acer griseum

Paperbark Maple

Paperbark Maple is a deciduous tree which grows to 25 feet with a narrow rounded crown. The branches angle up and out from the main trunk. Reddish bark peels away in paper thin sheets. Leaves have three leaflets and turn bright clear red and orange in autumn. Requires average water to become established.

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ACCENT TREE AT HETCH HETCHY RIGHT OF WAY

Cercis occidentalis

Western Redbud

Western Redbud is a small, flowering deciduous tree with a rounded crown which grows to 18 feet. It is native to California foothills below 4,000 feet. In spring, dense clusters of 1/2" long magenta flowers emerge before the leaves appear. Leaves are blue-green, and notched or rounded at the tip, turning light yellow or red in the fall. Needs no water after the third year.

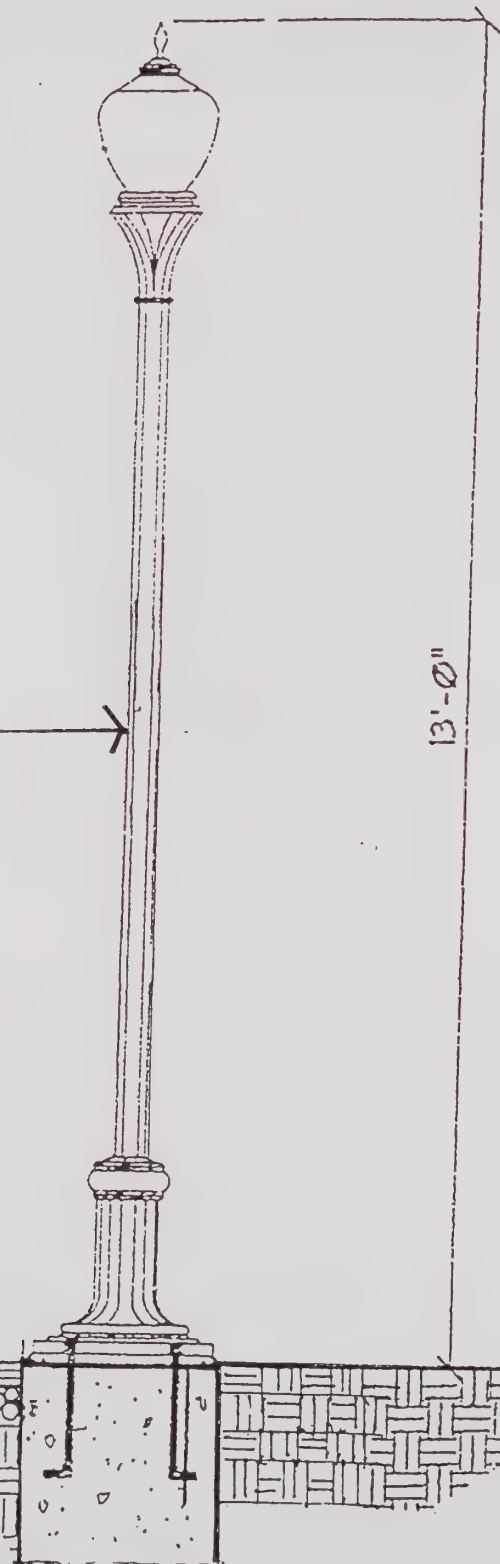
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APPENDIX B

TYPICAL DETAILS

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CITY OF MOUNTAIN VIEW STANDARD
BLACK CAST ALUMINUM FLUTED
POLE WITH 70 WATT FIXTURE
AND HIGH PRESSURE SODIUM
LAMP, BY WESTERN LIGHTING OR
APPROVEDEQUAL

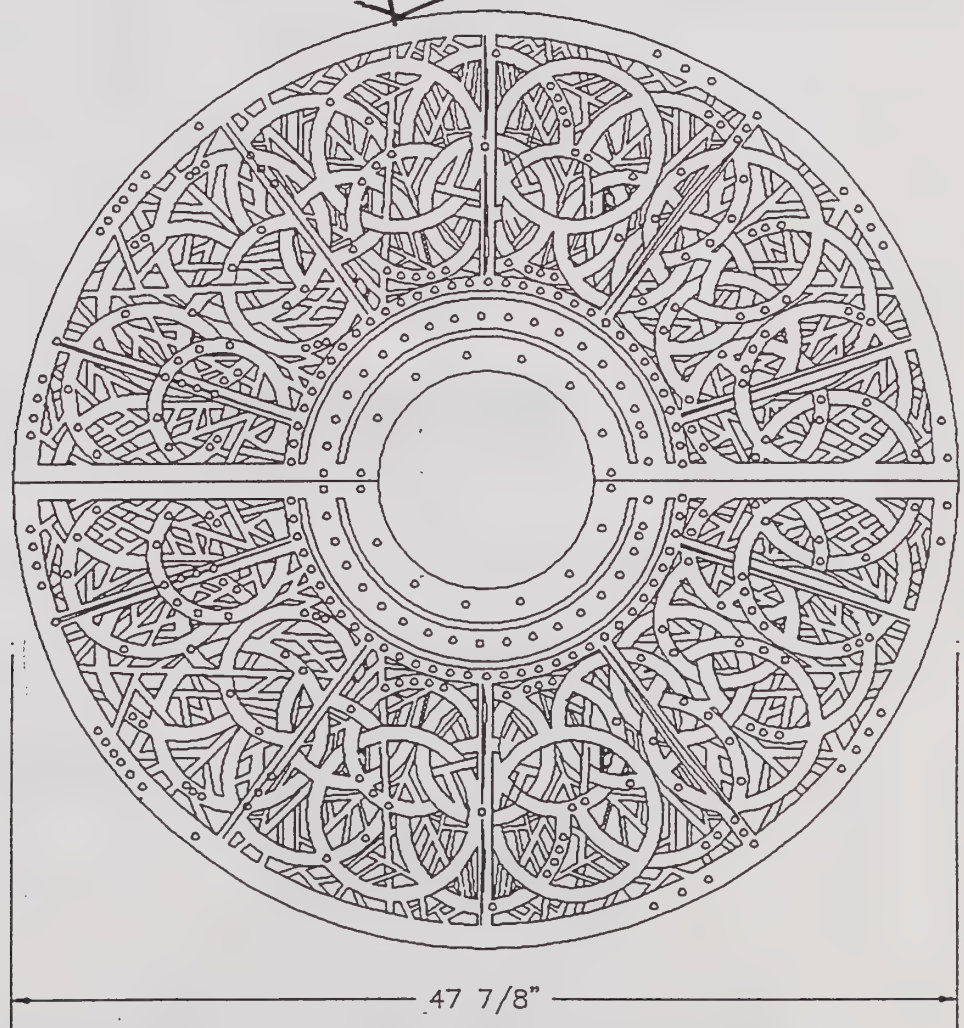


POST CAP STREET LIGHT DETAIL

SCALE: 1/2" = 1'-0"

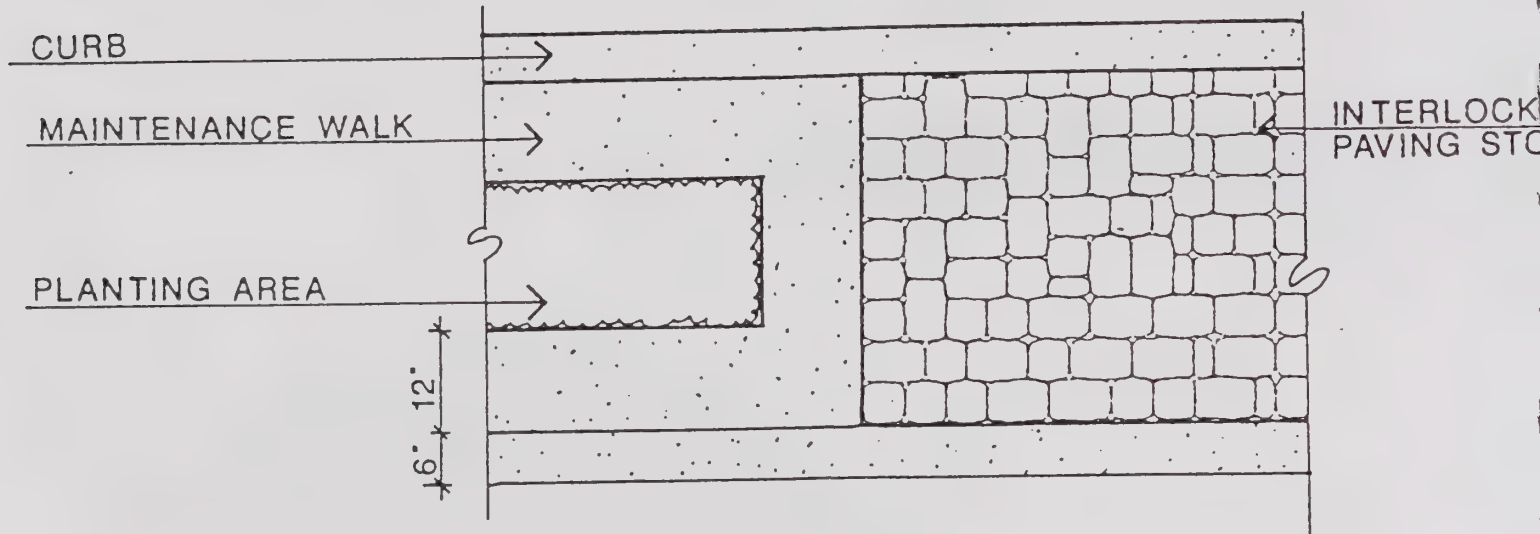
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48" DIAMETER ROUND STEEL
TREE GRATE MODEL #4' OT-TITLE 24
BY 'URBAN ACCESSORIES' OR APPROVED
EQUAL.

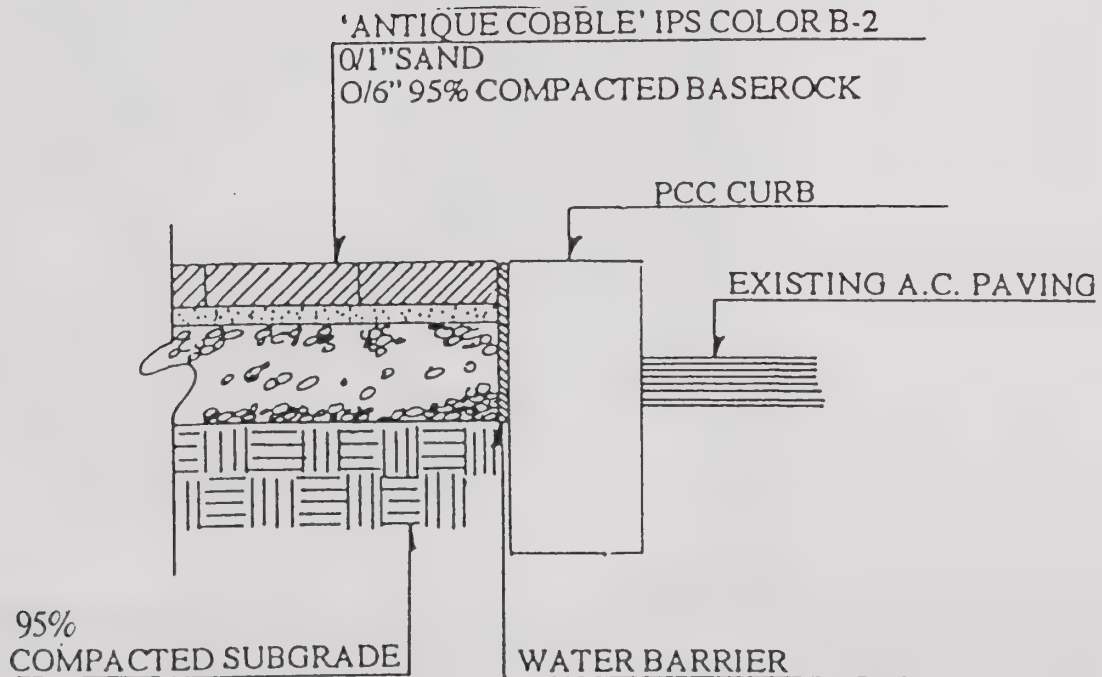


TREE WELL DETAIL
SCALE: NONE

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PLAN

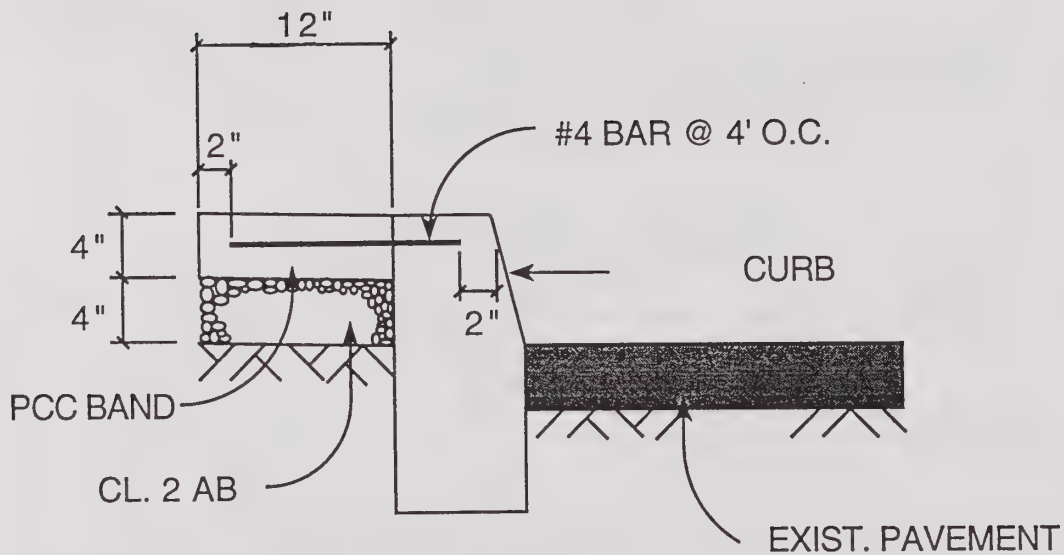


SECTION

MEDIAN NOSE PAVING DETAIL

SCALE: NONE

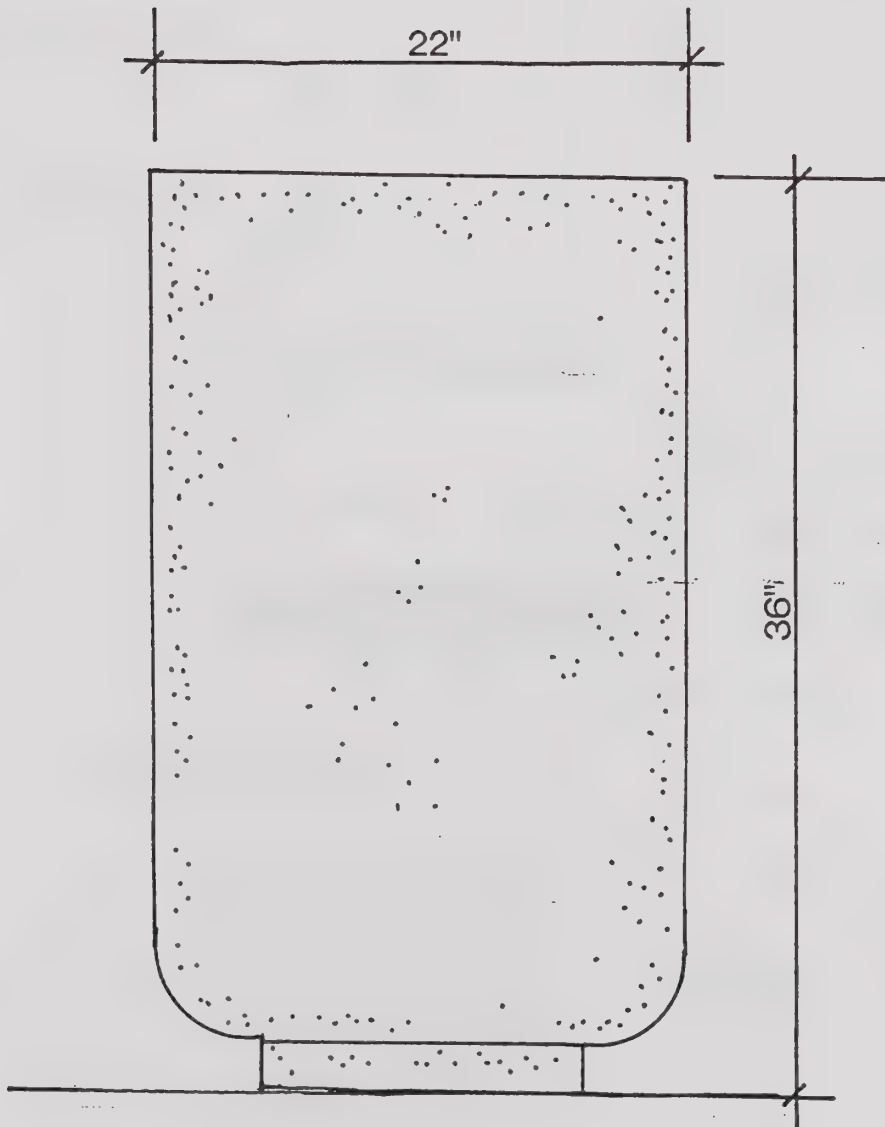
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CONCRETE MAINTENANCE BAND DETAIL

SCALE: NONE

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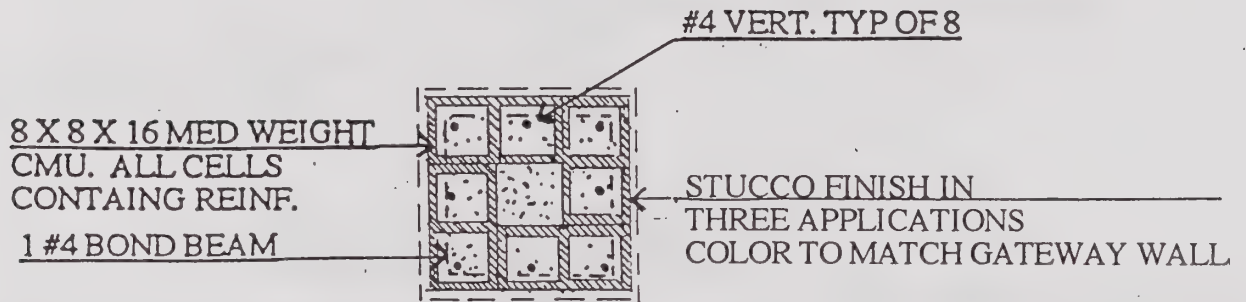


TRASH RECEPTACLE TO BE
'DURA ARTSTONE' MODEL NUMBER TR-D ROUND
LIGHT SANDBLAST FINISH
COLOR: SAND BUFF
WITH BLACK ANODIZED ALUMINUM LID:

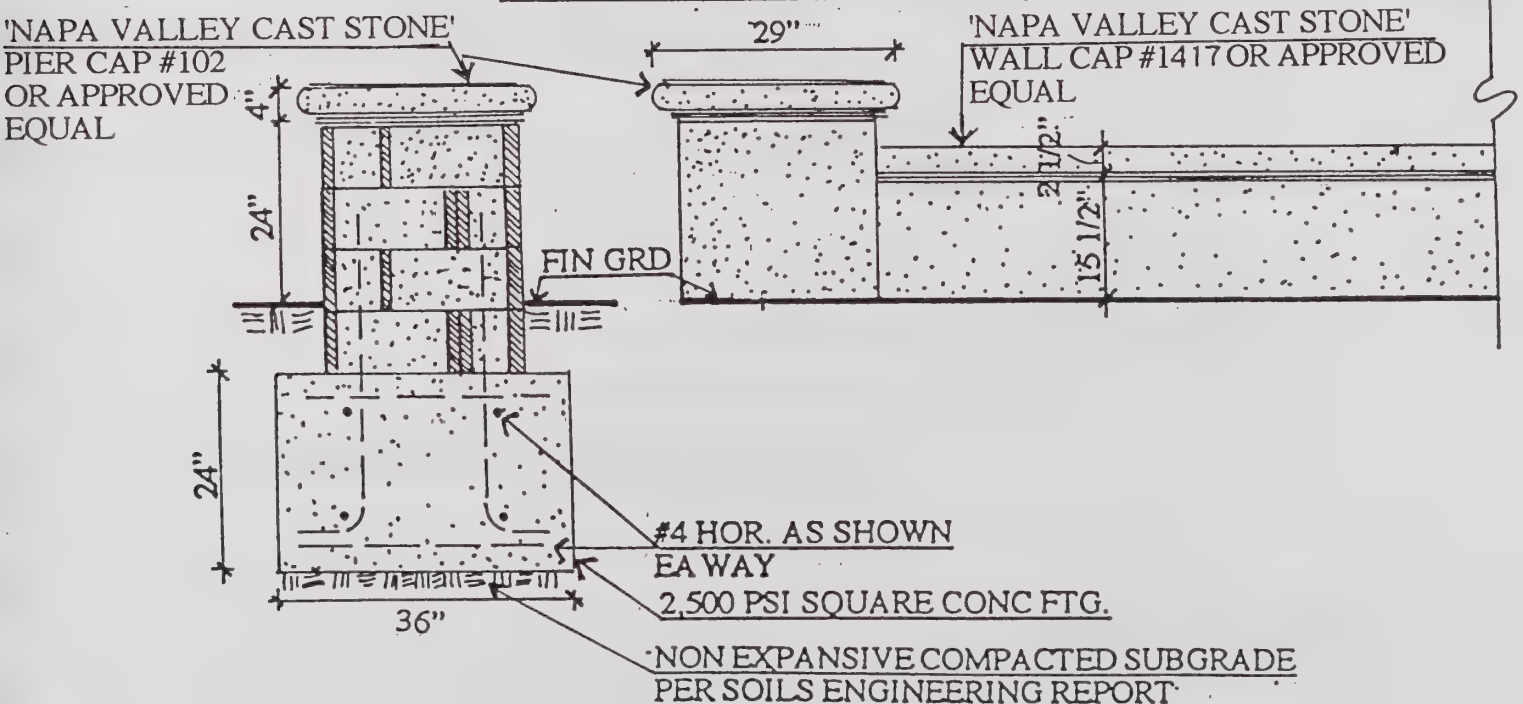
TRASH RECEPTACLE DETAIL

SCALE: NONE

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PARTIAL PLAN



SECTION

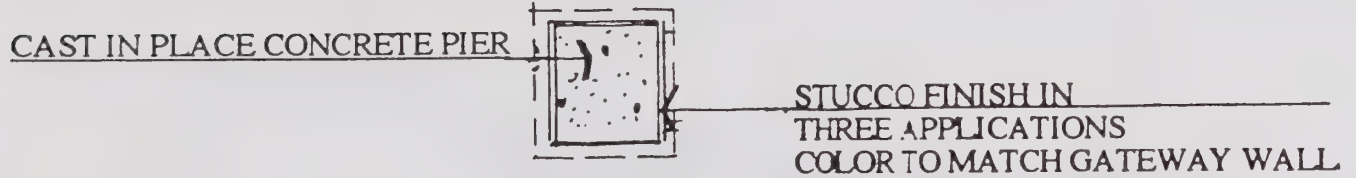
GENERAL NOTES

1. ALL PRECAST ITEMS AVAILABLE FROM: NAPA VALLEY CAST STONE, P.O. BOX 5539, NAPA, CA 94581 (707) 258-3340. COLOR: #155W; FINISH: MEDIUM ETCH

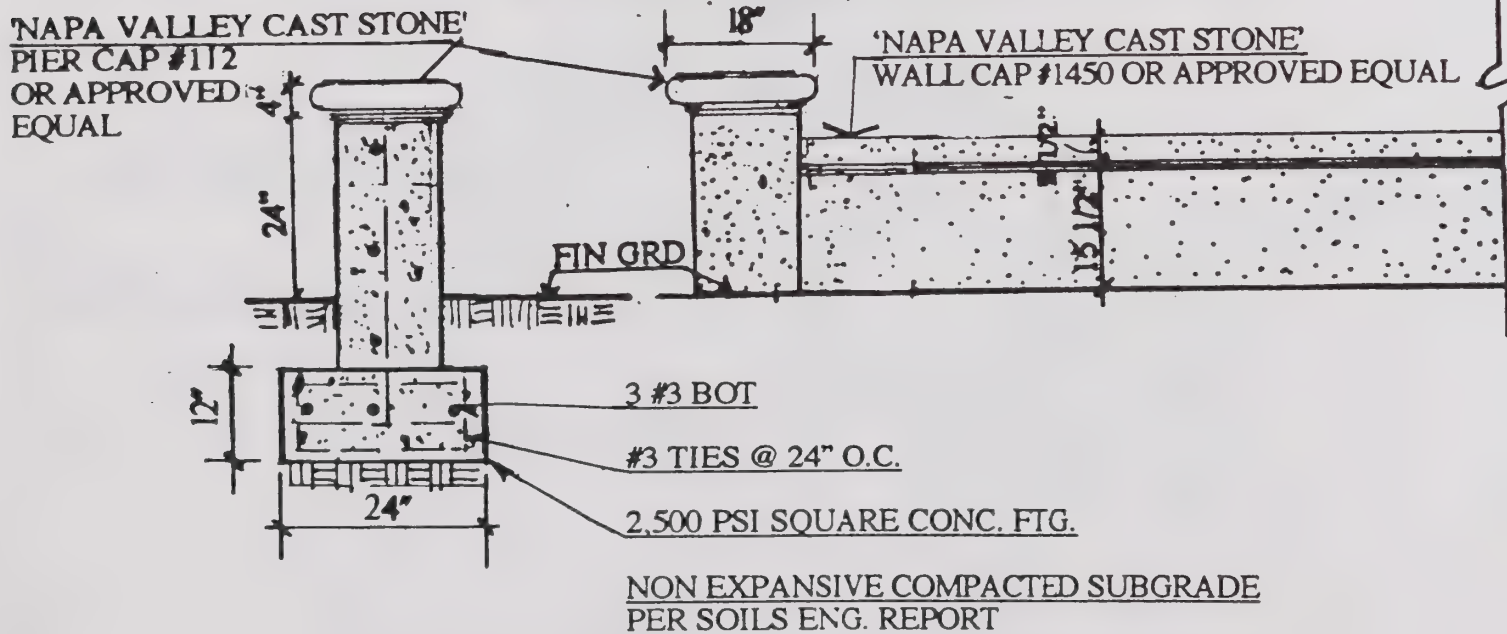
SEAT WALL DETAIL

SCALE: 1/2" = 1'-0"

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PARTIAL PLAN



SECTION

GENERAL NOTES

1. ALL PRECAST ITEMS AVAILABLE FROM: NAPA VALLEY CAST STONE,
P.O. BOX 5539, NAPA, CA 94581 (707) 258-3340. COLOR: #155W;
FINISH: MEDIUM ETCH

LOW WALL DETAIL

SCALE: 1/2" = 1'-0"

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